

WARD: Redland

SITE ADDRESS: 102 Gloucester Road Bishopston Bristol BS7 8BN

APPLICATION NO: 21/03767/F Full Planning

DETERMINATION DEADLINE: 18 January 2022

**DEADLINE:**

**Construction of 17 apartments following part demolition of building replaced with new build and conversion of existing first floor and loft spaces. Retention of retail at ground floor. (Major).**

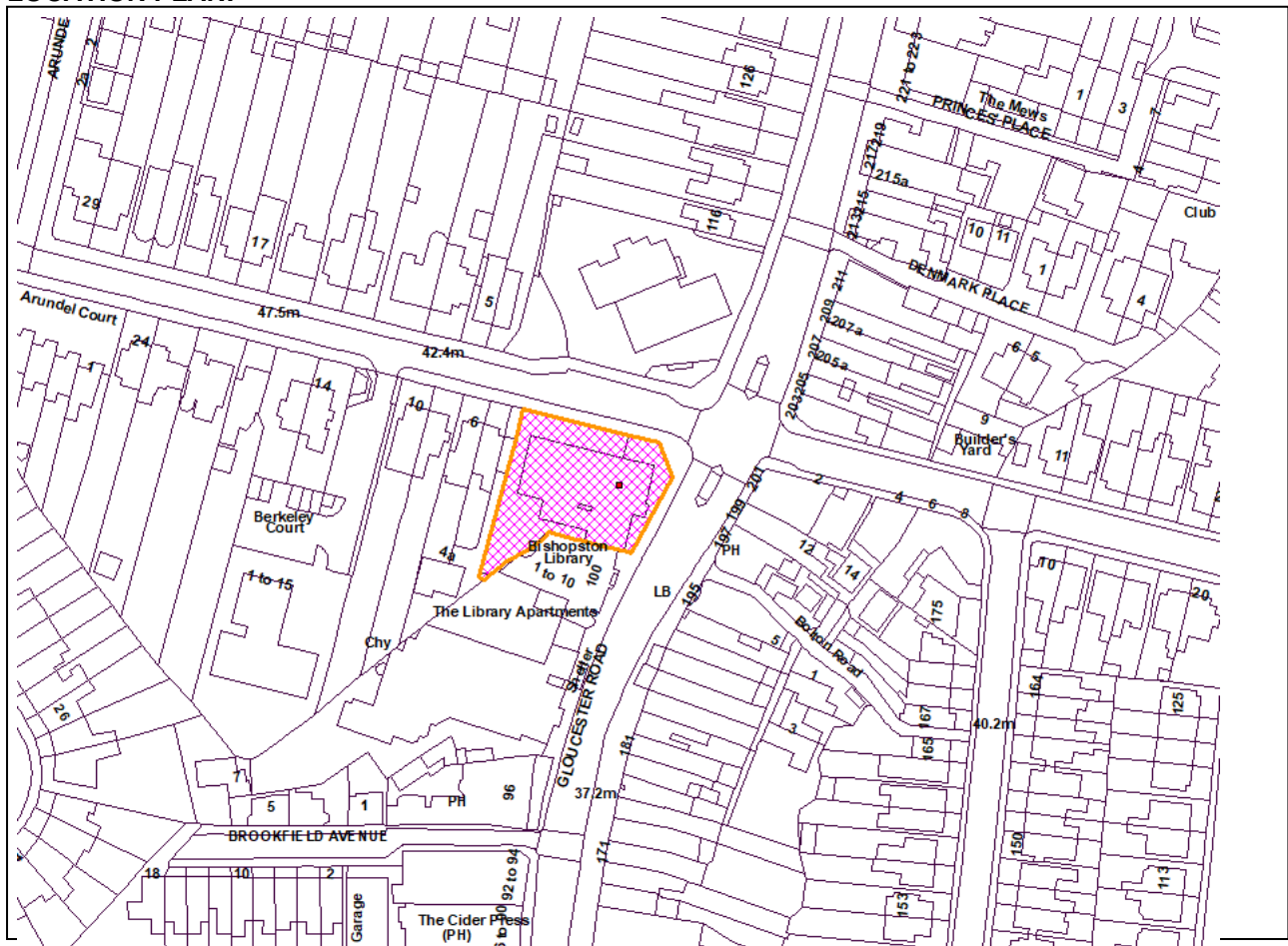
RECOMMENDATION: Refuse

AGENT: Nicholas Morley Architects  
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Corum Office Park  
Crown Way  
Warmley  
Bristol BS30 8FJ

APPLICANT: Nailsea Electrical  
102 Gloucester Road  
Bishopston  
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BS7 8BN

*The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.*

**LOCATION PLAN:**



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**21/03767/F- 102 GLOUCESTER ROAD**

**UPDATE FOLLOWING DEVELOPMENT CONTROL COMMITTEE B – 10 MAY 2023**

**BACKGROUND**

This application was considered by Development Control (DC) Committee B on the 10<sup>th</sup> May 2023 and a vote was taken to refuse the application following an officer recommendation for approval.

The application was for the construction of 17 apartments following part demolition of building replaced with new build and conversion of existing first floor and loft spaces with retention of retail at ground floor. The officer report to that meeting is appended to this report.

Committee voted (3 against the officer recommendation and 2 in favour of the officer recommendation) that an update report be provided by officers setting out grounds that could be legitimately used to refuse the application. The reasons provided by Members were:

1. Overdevelopment
2. Highway safety due to traffic movements in this busy location
3. Quality of the living environment specifically the limited aspect for a number of flats

This report provides officer advice on these reasons and drafts possible wording for these refusal reasons.

**OVERDEVELOPMENT AND QUALITY OF LIVING ENVIRONMENT**

During the Committee, concerns were raised regarding overdevelopment of the site. In the discussion these concerns related to the quality of the living environment created and specifically the proportion of single aspect units and the limited aspect of some flats. It is the advice of officer that points 1. and 2. above should be considered as a single reason.

In considering this issue, Members are again reminded that approved scheme 20/00022/F remains extant for the site for commercial space and 9 residential flats, which forms a material planning consideration and could be implemented (see approved plans within the Supporting Documents). The current proposal is for almost exactly the same floor layout within the proposed new build extension as that approved in terms of the proportion of single aspect units. The approved scheme included one additional dual aspect flat at second floor level only. The additional 8 units proposed would be achieved through 4 new units at ground floor (approved was commercial space) on an almost matching layout as already approved at first floor; and 4 units through converting the former chapel building at first floor (3 flats) and second floor (1 flat).

Of the 5 flats within the former chapel building, all homes would meet or exceed the national space standard for the number of bedrooms, taking into account the standard's requirements regarding head height and bedroom sizes. The relevant national space standards have been met for all units.

The policy in respect of providing dual aspect units encourages "the provision of dual aspect units, where possible, particularly where one of the aspects is north facing" (Policy DM29: Design of New Buildings of the Development Management Policies) and does not set specific standards to be achieved in that respect.

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As set out in the previous report (appended) - within the proposed development, 13 units would be single aspect (76%). Of these, 8 flats would be south or east-facing and 5 flats would also have access to a small garden or balcony, improving the overall living environment. The former chapel building presents various constraints (including heritage reasons) to creating dual aspect units in this case.

The outlook of the majority of units would be good and comparable to existing neighbouring homes in the area. Overall, only 4 of the proposed units could be considered in any way to have a more limited aspect (or outlook) by reason of proximity to existing boundary walls - these are Flats 12 and 13 at ground floor and Flats 3 and 4 at first floor. Flats 3 and 4 are almost identical layouts to those already approved under permission 20/00022/F.

Flats 12 and 13 would be new units compared to the approved scheme, as they are ground floor units and under that permission only commercial was approved at ground floor level. These units would have window to boundary fence/ wall measurements of 2.8m and 3.7m with external terrace areas of 17sqm and 25sqm respectively. The window to boundary distances would be comparable to other similar approved developments in the city and the private external amenity space provision would be considered a good amount of space comparable to the size of the units and would provide for everyday activities such as drying laundry, relaxing and play - comparable to many smaller terraced houses typical in Bristol. While these units would be bounded by the high boundary wall of The Library development (refer to Proposed West elevation), such situations are not unusual in urban environments and are often found in historic environments around the city where the topography leads to stepped forms of development creating a courtyard character.

A point was raised in the Committee's discussion around density compared to other parts of the city. It was reported that Core Strategy Policy BSC20 seeks a minimum indicative net density of 50 dwellings per hectare (dph) and the application proposal was cited to be 95 dph. The relative densities of other areas of the city were highlighted by the case officer during the meeting (85dph for terraced housing in Southville and 120dph for terraced streets in Totterdown). It is worth pointing out (though it can carry only very limited weight due to its status) that the Draft Local Plan Review (Consultation version March 2019) Draft Policy UL2: Residential Densities suggests for major development, minimum densities of 100dph in the Inner Urban Area (and 120dph in the Inner Urban Area More Intensive identified locations such as this site) to ensure the efficient use of land. As highlighted at the meeting, density figures are generally used as a guideline (except the minimum standard of 50dph) while considering all of the urban design considerations of a development.

To summarise, it is for the reasons above that officers do not consider overdevelopment and the quality of living environment and aspect/ outlook to be a justifiable reason for refusal and would urge Members to reconsider this.

It is also worth noting that the extant permission, being only 9 units, did not meet the threshold (10 units) for affordable housing. The current scheme at 17 units does meet the threshold and includes the provision of 4 no. First Homes.

However, if Members are still minded to refuse the application on this basis, it is recommended that points 1 (overdevelopment) and 2 (living environment) above be combined to form a single reason for refusal to substantiate the specific harm arising from overdevelopment of the site.

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**HIGHWAY SAFETY**

The Transport Development Management representative has again visited the application site and has again advised that in their opinion there is no grounds for a road safety objection given that Berkeley Road has a generous carriageway at the junction with Gloucester Road.

Speed restrictions on Berkeley Road and Gloucester Road are 20mph meaning low traffic speeds and that manoeuvring into and out of the spaces can be safely achieved and already are carried out in this way. Off-street parking already exists in the same location i.e. there is no change.

The servicing of the petrol station (via tanker) is one-way movement of a maximum of 2 trips a week. The evidence provided by the applicant showed the movement into the petrol station can be made under the proposed scheme. This application has removed the off-street parking which allows for a better footway.

The evidence indicated there were potential options to increase on street parking provision that could be used for loading only and therefore benefit any residence in the vicinity not just the new development. However, overall the proposal would be considered by officers to be acceptable without any additional on-street parking being created by reason of the highly sustainable location (considered one of the most sustainable locations in the city). New residents would not be eligible for permits in either existing or future parking controlled zones.

The submitted general arrangement plan was indicative as previously explained and it would be subject to technical approvals and traffic regulation orders which are highway authority responsibility.

Overall, in road safety terms this is safer than the previously approved scheme. However, the section 278 agreement would require a safety audit to ensure all changes to the public highway meet our highway authority standards. Transport objections have to be deemed severe or insufficient mitigation has been provided to be upheld and in their view, this is not the case.

Paragraph 111 of the National Planning Policy Framework states that that “Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.”

Officers therefore strongly advise that we do not consider there to be grounds for refusal on this basis. However, if despite this advice, Members remain minded to refuse the application the refusal reason set out below would be advised.

**AFFORDABLE HOUSING**

While the applicant and officers have agreed a policy compliant approach to affordable housing provision following Viability Assessment, through the provision of 4 no. First Homes, a section 106 agreement is not yet in place to secure delivery of these dwellings.

Therefore, Members should be aware that by refusing the application, the opportunity to secure these affordable homes as agreed may be lost. The absence of an agreement to

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secure these homes must therefore form an additional refusal reason- see recommended wording below.

The applicant has advised that they would appeal a refusal of this application and should this be the case, the Inspector may be minded to allow this reason for refusal to be addressed via the agreement of a section 106 at that stage.

**TILTED BALANCE**

Any decision taken must demonstrate that the refusal reasons (the adverse impacts of the development) would significantly and demonstrably outweigh the benefits of the development, when assessed against the policies in the National Planning Policy Framework (NPPF) taken as a whole.

**POSSIBLE REASONS FOR REFUSAL**

Noting the recommendations of officers above, if Members consider that the proposed development would remain unacceptable, the following reasons for refusal would be recommended.

1. The proposal would constitute an overly cramped form of development of the site resulting in overdevelopment by reason of the proportion of single-aspect flats, the limited aspect for a number of flats and the overall quality of the living environment. This would be contrary to paragraph 130 of the National Planning Policy Framework, Policies BCS20 and BCS21 of the Bristol Local Plan: Core Strategy and Policies DM27, DM29 of the Bristol Local Plan: Development Management Policies.
2. The proposed development would result in unacceptable highway safety conflicts as a result of vehicle manoeuvring into and out of the proposed off-street car parking spaces in this location, which experiences high vehicle traffic and pedestrian movements by reason of the location adjacent to the junction of Berkeley Road and Gloucester Road, the adjacent pedestrian crossing and the close proximity to the petrol filling station. This would be contrary to Policy BCS10 of the Bristol Local Plan: Core Strategy (2011) and Policy DM23 of the Bristol Local Plan: Development Management Policies (2014).
3. In the absence of an appropriate agreement under s106 of the Town and Country Planning Act 1990, the proposed development fails to provide for Affordable Housing requirements in order to mitigate the impacts of the development contrary to Policy BCS11 of the Bristol Local Plan: Core Strategy (2011) and Policy BCS17 of the Bristol Local Plan: Development Management Policies (2014).

**Development Control Committee B – 10 May 2023****Application No. 21/03767/F : 102 Gloucester Road Bishopston Bristol BS7 8BN****REASON FOR REFERRAL**

Local ward Member for Redland Ward, Councillor Fodor has referred this application to Development Management Committee should it be recommended for approval by officers for the following reasons:

This major application, on the site of a previous permission granted, now seeks to build 17 flats where previously 9 had been approved just a year earlier following extensive negotiations.

There are residents' concerns regarding privacy due to proposed balconies overlooking the adjacent Bishopston Library flats and concerns over parking, and concerns about overdevelopment being attempted. These issues need to be debated by committee in a public meeting where statements can be heard from residents and issues considered before any conditions are agreed or the decision gets made.

**SUMMARY**

The application refers to the redevelopment of the former Nailsea Electrical premises at 102 Gloucester Road in Redland ward. The site comprises a retail unit located within a former Methodist Chapel (a locally listed building and heritage asset) and its modern extension, with an external yard to the rear. Off-street customer parking lies perpendicular to Berkeley Road. The site is within the Gloucester Road Conservation Area.

A recent planning permission for redevelopment of the site for retail, office and residential use (9 flats) remains extant. This included parking for the commercial units on Berkeley Road and retail use across the whole ground floor plus extension over the rear yard. The retailer Nailsea Electrical has since relocated to Hengrove and a new application proposal has been brought forward.

The current application proposal comprises retail use at the ground floor and residential units at first floor of the former chapel within a new-build extension to the west of the site containing residential units at ground, first and second floor levels. There are a total of 17 residential homes proposed comprising 7 no. 1-bedroom flats, 7 no. 2-bedroom flats and 3 no. 3-bedroom flats. Affordable housing is proposed in the form of 4 'First Home' units (all 1-bedroom flats).

The rear yard is proposed as external landscaped space also housing the residential cycle store and air source heat pumps (ASHPs). 4 vehicle parking spaces are proposed accessed from Berkeley Road- 3 no. for the retail use a 1 no. disabled parking bay dedicated for the residential use. Waste storage and access would be from the Berkeley Road frontage.

The response to publicity and consultation on the application can be summarised as: 28 contributors commented on the application with 20 objectors, 5 contributors making general representations and 2 in support. These include an objection from The Bishopston Cotham and Redland Street Scene Group and The Conservation Advisory Panel (neutral comment).

Objection comments relate primarily to overdevelopment, density, lack of parking and traffic issues, impact on neighbouring residents, living environment for future residents, proposed materials, lack of affordable housing provision, landscaping and construction impacts. Comments in support relate to the overall improvement of the proposal to the site and area including restoration of the dilapidated historic building, removal of the unsightly scrap yard and replacement with a garden and good cycle storage.

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The officer's summary assessment of the proposal is as follows:

The National Planning Policy Framework (NPPF) paragraph 11 outlines a presumption in favour of sustainable development. For decision taking this means "approving development proposals that accord with an up-to-date development plan without delay" unless the proposal would be contrary to policies in the NPPF relating to designated assets (heritage assets or habitat sites) or the adverse impacts of the development would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

In relation to this, the Council has a clear identified shortfall in housing supply and officers advise that the proposals would be in accordance with the NPPF in relation to designated heritage assets.

The principle of the redevelopment of the site has already been approved by the extant permission including the proposed new-build 'envelope'- the size of the proposed new-build structure would reduce under the current proposal. Officers advise that the proposal would be an acceptable mix of housing, including affordable housing, and makes appropriate provision for the amenity of future residents. The amenity of neighbouring residents would be safeguarded, and the proposal includes a reduced number of balconies compared to the extant scheme. The local concern regarding the lack of parking is noted, as is the demand for parking in this area. However, the proposal is in a highly sustainable location, to which car-free development is directed and is a location where residents could reside without the need for a private vehicle. The addition of a number of additional vehicles to the area for parking could be accommodated without unacceptable impact on highway safety and two on-street parking spaces would be created. All other material considerations have been addressed.

Officers therefore advise approval of the application subject to planning conditions and planning agreement.

## SITE DESCRIPTION

The development site is 102 Gloucester Road, the Former Methodist Chapel, currently a retail store (use class A1/E) previously occupied by Nailsea Electrical. The site is located to the western side of Gloucester Road at the Berkeley/Sommerville Road crossing.

The former chapel has an L-shaped footprint extending along both Gloucester Road and Berkeley Road. The Gloucester Road elevation includes a distinctive clock tower including main entrance. To the western, Berkeley Road side the building includes a modern side storey extension constructed with red brick and flat roof. A yard area extends to the rear (south-west) of the site. The building is presently accessed via Berkeley Road and the submission sets out that there are 10 off-street parking spaces located perpendicular to the road in a forecourt area on this side of the site.

The Bishopston Library mixed use redevelopment is located immediately to the south of the site. Dwellings to the southern side of Berkeley Road commencing at 4 and 4A Berkeley Road are located immediately to the west. Gloucester Road (A38) is a major arterial route north from the city centre. At this point it is lined by predominantly commercial uses, frequently with residential accommodation above. Residential becomes the predominant use within side streets, as is the case on Berkeley Road. The site is located within the designated Gloucester Road Town Centre. The site is also located within the Gloucester Road Conservation Area. The Former Methodist Chapel is designated an Unlisted Building of Merit and features on Bristol's local list of valued buildings.

## RELEVANT PLANNING HISTORY

**Development Control Committee B – 10 May 2023****Application No. 21/03767/F : 102 Gloucester Road Bishopston Bristol BS7 8BN**

20/00022/F- Demolition of western part of former chapel and single storey extension and construction of three storey extension comprising 9 dwellings (use class C3), extension of ground floor retail unit (use class A1), conversion of existing loft to office use (use class B1a), external alterations, associated access and landscaping works. GRANTED 15.02.2021

19/01527/F- Construction of 9no. (2-bedroom flats) flats over extended ground floor retail area, following part demolition of ground and first areas, and conversion of loft area to create office space. External alterations to existing building and forecourt on Gloucester Road and Berkley Road Elevation including roof extension to tower. WITHDRAWN 02.07.2019

**APPLICATION**

The current application seeks full planning permission for the following proposed development:

- Part demolition of the existing building;
- Conversion of the former chapel building and a new build extension;
- Retail unit at ground floor within the former chapel building;
- A residential mix of 17 flats comprising 7 no. 1-bedroom flats, 7 no. 2-bedroom flats and 3 no. 3-bedroom flats.
- Of these 17 units, 4 First Homes would be provided (4 no. 1-bedroom homes).
- 4 car parking bays are proposed to the Berkeley Road frontage of the site (in place of existing parking bays). 3 for the retail units, 1 disabled bay for the residential flats.

In terms of the proposed design, a series of four parallel gable roofs facing Berkeley Road are proposed. The extension would be constructed with pennant stone to match the chapel at ground floor level, limestone banding courses and window surrounds, with red brick to the upper floors. Alterations to the chapel building would include the removal of all current retail signage, introduction of full height windows within the Gloucester Road elevation, reinstatement of the clock tower roof, original window openings and entrance, roof windows to Gloucester Road and dormer windows, reinstatement of Berkeley Road windows and new entrance doors. All proposed windows and doors would include dark grey metal frames. Dormers would be clad with zinc. The chapel would be re-roofed with dark red double roman tiles.

Access to the proposed flats would be via entrances within the Berkeley Road elevation. These would include entrance canopy and glazed access doors accessed by walled and paved footpaths from Berkley Road. Cycle parking and refuse stores would also be accessed via separate service entrances in this location. Waste collection points are proposed to the front of the building in this location. Bins and recycling would be collected from this location on collection day however bins would be permanently housed within the building. Cycle parking for a total of 26 bikes within a bike store at the rear of the property accessed via the side access from Berkeley Road. It is proposed to reintroduce primary access to the retail unit via the main former chapel entrance on Gloucester Road. The existing retail entrance on Berkeley Road would also be retained.

Landscaping and boundary treatment improvements are proposed to the road frontages. The footway on Gloucester Road would be resurfaced with paving to match that adjacent at the Bishopston Library development. This would extend to the junction with Berkeley Road. The footway on Berkeley Road itself will be resurfaced with tarmac with new pennant kerbs where raising kerbs is required. It is proposed to remove an existing young street tree and replant a new tree further west on Berkley Road outside the site including installing a new tree pit.

The development would feature on site renewable energy generation in the form of a solar



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photovoltaic array installed to the pitched roofs of the proposed three storey extension and air source heat pumps within the rear external space.

**AMENDMENTS TO THE APPLICATION**

The application has been amended during the course of the application and further information has been submitted in support of the application. This includes the following:

- Disabled parking bay included for residential units;
- Parking outside ground floor flats (Berkeley Road) removed and replaced with private gardens with landscaping/ tree planting and access;
- Rear (southern) elevation amended to omit second floor balconies and balconies reduced at first floor;
- Waste store moved from rear garden to dedicated internal store accessed from the Berkeley Road frontage;
- Viability assessment (affordable housing);
- Sustainability statement updated and Overheating Assessment provided. Air source heat pumps shown on plan (to rear garden);
- Noise assessment (acoustic report) updated;
- Bat survey updated;

**PRE-APPLICATION COMMUNITY INVOLVEMENT**

Due to its size, the application is required to be accompanied by a Statement of Community Involvement. Guidance and good practice examples exist to inform the choice of appropriate methods in order to help ensure effective, efficient, transparent and accountable community involvement. Those responsible for undertaking community involvement are expected to reflect such good practice to ensure inclusive, fair and effective initiatives. Failure to do so may limit the validity and relative credibility of the involvement undertaken.

The applicant prepared a statement of community involvement (dated 5<sup>th</sup> July 2021) which has been assessed, and is summarised below:

## i) Process

Covid restrictions meant that post box deliveries and social media were used to inform the community of the revised proposals. A letter drop was made on 5th March 2021 to over 100 properties to explain that the proposal was to increase the current approval for 9 residential units to 14 residential units whilst maintaining retail areas in existing building. Further to this a "whatsapp" drop was made on 9th May 2021 which updated the interested parties and neighbours (the whatsapp group has a number of members, so it was felt this was a reasonable approach) which stated that after further consideration it has been decided to resubmit for 17 apartments and retain retail use on the ground floor of the existing building. The flyers for the proposals are included in Appendix A. The property owners have also liaised with neighbours over the last few months so they are aware of the revised proposals.

## ii) Fundamental Outcomes

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Responses received were listed as follows: landscaping, construction including timescales, overdevelopment, car parking and hard landscaping, climate change, use as community space, bins and recycling, terraces to ground floor. The applicant responds to each point in the Community Involvement Statement.

**RESPONSE TO PUBLICITY AND CONSULTATION**

The application was publicised by site notice, press notice and letters to individual neighbours (92 properties). In total (over two rounds of consultation), 28 contributors commented on the application with 20 objectors, 5 contributors making general representations and 2 in support. The first period of consultation was carried out in October 2021. 18 representations were received- 14 objections, 3 neutral comments and 1 comment in support of the proposal, as summarised below:

*Objections:*

- No parking provision for residents or provision for deliveries;
- Excessive density;
- Impact on neighbouring properties- light and privacy- especially balconies to rear;
- Concerns regarding management of waste;
- Clarity sought over landscape boundary treatment to adjacent residential property and landscaping generally to soften the landscape and improve the flora, also the 'sedum roof' previously proposed to the rear extension;
- It is set out in the submission that construction works would require pavement closure for 18 months- this would prevent pedestrian access and be unsafe;
- Wildlife impacts of development;
- No consultation \*\*\*

*Support:*

- An adjacent resident is keen to see the existing unsightly scrap metal yard removed. The proposal to replace the (previously approved) ground floor commercial area with residential is supported in relation to those existing residents at the eastern end of Berkeley Road;
- The addition of a garden area is a positive change provided well-maintained in perpetuity;
- Support the good-sized area for bins/ cycle store to the rear of the site;
- Restoration of a dilapidated building;

*Neutral:*

- Generally supportive of the design of the north elevation though seek soft landscaping to the north elevation. Object to the south and west facing balconies to first and second floors. Soft landscaping to be clearly specified. Parking concerns.

Following the amendments to the application set out above, a second period of consultation was carried out in March/ April 2023. 14 representations were received (from 11 objectors and 1 supporter). The comments received are summarised as follows (for full comments please refer to the public website):

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- Proposal is almost double the number of flats of the previous approved scheme;
- High density and cramped;
- Negative impact on neighbours including light and privacy;
- Overbearing and oppressive, building block too deep in relation to 4a Berkeley Rd;
- Poor quality living environment of proposed dwellings- single aspect and north-facing;
- Design: red brick is not typical of Berkeley Road; render would be preferable. Glazing excessive to some flats;
- No parking proposed, no car club provision and poor public transport with addition of 17 to 34 extra vehicles.
- The site is just outside the existing Residential Parking Zone (RPZ) and already affected by pre-existing highway problems on Berkeley Road including overparking, dangerous parking, speeding, commuter rat-run, dangers to pedestrians including school children and pollution; particularly near junction with Gloucester Road- the proposal would add to this;
- No affordable housing provision;
- Inadequate greenery- garden and hedges should be required;
- Construction must ensure pedestrian movement;
- Inconsistencies in drawings; \*\*\*\*

*Support:*

- The proposed development would be an improvement to the site and area;

## LOCAL AMENITY GROUPS

The Bristol Tree Forum (summarised- please see full comment online- 6 Oct 2022) submitted an enquiry regarding the application and the Heras fencing erected around the site and commenting that the recently-planted street tree outside the site that they had fought to save was still looking damaged. The Arboricultural Impact Assessment (AIA) for the application shows replacement of the tree further up the street and the BTF enquired as to tree officer comments on the application and whether the tree could be protected.

*The Bishopston Cotham and Redland Street Scene Group (full comment- 16 Aug 2021)- Objection to the application:*

“BCR SSG note that the principle of residential development on this site is already established by the approved application 20/00022/F. This revised proposal will create unacceptable standards of residential accommodation and we object to the application.

The change from a smaller number of individual units with waste and recycling storage on the front of the building to a much larger number of flats with a poorly-located bin store at the rear of this site is not acceptable. Good management of waste and recycling is important to reduce the negative impact of densification of residential areas; location of the bins and containers so that they are easily accessible will reduce the likelihood of bins and boxes being left on streets and of fly-tipping. This is a health consideration as well as a convenience issue as set out in DM32.

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We object to the retention of car parking bays in front of the residential accommodation for business use given the much-reduced floor area of the showroom and the moving of goods storage off site. This area should be used to make a more appropriate setting for ground floor residential accommodation as a buffer area from the street with appropriate planting including small trees, and to allow location of waste and recycling and cycle stores at the front of the building.

This location at this busy junction is not ideal for residential accommodation, particularly at ground floor level where air quality from particulates from traffic is poor, so more consideration to providing a standard of accommodation which does not contribute to mental and physical health issues for future residents must be given. DM14. Relocating the waste and recycling storage to the Berkeley Road front of the building will free up the ground level area at the rear of the building to create a private amenity space for the residents of the flats. Inclusion of tree and shrub planting will improve the air quality for residents at this busy junction where vehicles wait at traffic lights. Access to the rear open space from the residential accommodation is poor. Consideration to creating a direct access from the hall and stairway should be given.

The depth of the proposed residential building means that a number of North-facing single-aspect flats are created. This is unsatisfactory and contrary to the provisions of Policy DM 29 which states that new residential development should provide dual aspect where possible particularly where one of the aspects is north facing. The proposed layout includes a particularly unacceptable flat at ground floor level which is single aspect north facing with bedrooms facing towards the street which does not comply with aim for active frontages, natural surveillance, and appropriate levels of privacy, outlook and daylight. DM 29. The outlook for residents is onto the parking bays for visitors to the shop so windows will look onto visitors and delivery vehicles, which will be coming and going from the parking bays all day. This will exacerbate the already polluted air. The proposed development will fail to promote and enable a healthy living environment DM 14. The 2nd floor level flats in the roof of the chapel above the retained showroom section are very poor-quality residential accommodation. The narrow dormer windows with solid cheeks will give a very low level of natural light and ventilation and one of the two units is another single aspect north-facing unit. The other unit faces east onto Gloucester Road and again is a single aspect unit. The proposed roof lights will not make up for this poor-quality lighting and ventilation provision nor the restricted views available from the narrow dormer windows. We note that one of these flats are already included in the approved scheme. We cannot see how this conforms with DM14 or DM29.”

*The Conservation Advisory Panel commented as follows (full comment 29 Aug 2021)- neutral response to the application:*

“The Panel recognises that there is an existing planning consent for this site and that externally only relatively minor changes to that consent are proposed. The principal contribution of this building to the Conservation Area is its form and massing. The proposed new dormers on the east side would interrupt the sloping roof on the Gloucester Road elevation and harm the building's appearance and should be omitted.”

**LOCAL COUNCILLOR CORRESPONDENCE**

As set out above, Local ward Member for Redland Ward, Councillor Fodor has referred the application to Development Management Committee (see reasons given above).

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## CASE OFFICER RESPONSES TO PUBLIC CONSULTATION

## INTERNAL CONSULTATION (BRISTOL CITY COUNCIL CONSULTEE ADVICE)

Please note that these are summarised comments and full comments can be viewed online for most responses unless stated otherwise:

**The Urban Design Team** advised (verbal comment given) that they would support the scheme on balance subject to conditions.

**The Conservation Officer** has commented as follows (full comment below): - I am satisfied that there is no greater heritage impact than the previously consented scheme 20/00022/F, so would support the proposals subject to the application of the conditions previously applied.

The proposed dormer windows to be introduced on the north roof slope of the former chapel do have a degree of impact on the architectural character of the historic building. The impact is very low and proportionate to bringing the building into a new viable use. The overall impact is considered offset by the public benefits of the scheme, preserving the building as a landmark within the Conservation Area. of restoration, adaptation, and reuse of the buildings, the provision of new residential accommodation, and the environmental benefits to the Conservation Area. Any approval should be conditioned to require detailed designs of the dormers and windows to ensure the appearance is appropriate, and minimise the impact as far as practical.

**The Housing Strategy and Enabling Team** advises that (summarised- full comment online): The applicant submitted a viability assessment which has been externally assessed. The external consultant concluded that 4 affordable housing units should be provided. In order to deliver affordable homes on site we are agreeable to 4 x 1b2p flats as First Homes. These are flat number 3, 4, 7 and 12.

**Nature Conservation Officer** (summarised) has raised no objection to the proposal and confirmed that the protected species survey is acceptable and up-to-date.

**The Pollution Control Officer** raises no objection to the proposals subject to conditions regarding noise insulation (summarised).

**Air Quality** has commented as follows (full comment):- No air quality assessment is submitted, but I have no objections on the grounds of air quality. In the most recent representative year, a nearby NO<sub>2</sub> monitoring site recorded an annual mean of 38.3 ug<sub>m</sub>-3 in 2019 (limit is 40). Future concentrations are likely to decrease further, so it is unlikely that new exposure will be introduced, especially as the proposed ground floor is retail, and the facade is further away from the road than the monitoring site. I have no objection to the development on air quality grounds.

**Flood Risk Manager** has commented as follows (full comment):- The drainage strategy proposed for this development is generally quite good. The discharge rate of 5l/s and storage volume of 51m<sup>3</sup> are fine. It seems Wessex Water have previously been consulted about this but confirmation of the sewer connection would be required. The use of permeable paving, a green roof and planters is also good, we would however like to view more detail of these if possible. As Wessex Water have confirmed acceptance of the proposed flow rates entering the sewer system applying the SuDS condition to cover the outstanding SuDS requirements would be acceptable. With the offsite discharge rate and outlet agreed the remaining details could be confirmed at a later stage through condition.

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EXTERNAL CONSULTATION (CONSULTEE ADVICE)

**Wessex Water** has input on the application and advised the applicant regarding the public surface water sewer connection.

**HSE - Fire Safety** has commented as follows:- We became a statutory consultee on 1st August 2021. We cannot comment on planning applications from local planning authorities submitted prior to that date (unless a subsequent application, after 1st August 2021, is made under section 73 of the Town and Country Planning Act 1990). Therefore, on this occasion we will not be able to provide a response to this application." (This case was validated on 27/07/21 according to the records provided).

RELEVANT POLICIES

National Planning Policy Framework – July 2021

Bristol Local Plan comprising Core Strategy (Adopted June 2011), Site Allocations and Development Management Policies (Adopted July 2014) and (as appropriate) the Bristol Central Area Plan (Adopted March 2015) and (as appropriate) the Old Market Quarter Neighbourhood Development Plan 2016 and Lawrence Weston Neighbourhood Development Plan 2017 and the Hengrove and Whitchurch Park Neighbourhood Development Plan 2019.

In determining this application, the Local Planning Authority has had regard to all relevant policies of the Bristol Local Plan and relevant guidance.

EQUALITIES

The public sector equalities duty is a material planning consideration as the duty is engaged through the public body decision making process.

"S149 of the Equalities Act 2010 provides that a public authority must in the exercise of its functions have due regard to:-

- (a) eliminate discrimination, harassment, victimisation and any other conduct prohibited under the Act
- (b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it
- (c) foster good relationships between persons who share a relevant characteristic and those who do not share it.

During the determination of this application due regard has been given to the impact of the scheme upon people who share the protected characteristics of age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The proposal will provide a mix of housing size and tenure to reflect identified need to include a number of smaller units, 4 of which would be categorised as affordable (First Homes). The access to the majority of dwellings will be at an acceptable gradient and a disabled parking space is provided. It is considered that there will be a positive impact on equalities.

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## KEY ISSUES

## (A) SUSTAINABLE DEVELOPMENT

The National Planning Policy Framework (NPPF, 2019) states that "the purpose of the planning system is to contribute to the achievement of sustainable development". This includes economic, social and environmental objectives. NPPF Paragraph 11 (c) and (d) relate to the presumption in favour of sustainable development in decision-taking (i.e. as opposed to plan-making). They state that decisions should apply a presumption in favour of sustainable development, which means:

(c) approving development proposals that accord with an up-to-date development plan without delay; or

(d) where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date (8) , granting permission unless:

(i) the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed (7) ; or

(ii) any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

Footnote (7) refers to assets of particular importance. In this case, the only relevant asset of those listed, is that of the Gloucester Road Conservation Area, a designated heritage asset.

Footnote (8) relating to the term 'out-of-date' states this includes "where the local planning authority cannot demonstrate a 5 year supply of deliverable housing sites (with the appropriate buffer, as set out in paragraph 74); or where the Housing Delivery Test indicates that the delivery of housing was substantially below (less than 75% of) the housing requirement over the previous 3 years." Please refer to Key Issue B '*Proposed residential use and housing delivery test*' for further assessment of this matter.

Local Plan Policy DM1 (Presumption in Favour of Sustainable Development) outlines that the city's approach to development proposals will generally be positive and reflective of the presumption in favour of sustainable development as referenced throughout the NPPF.

The considerations of whether the proposal would accord with the development plan and whether the development can be considered up-to-date are covered in the following Key Issues.

## (B) PRINCIPLE OF DEVELOPMENT – EXISTING AND PROPOSED LAND USES

*Proposed residential use and the Housing Delivery Test*

On 19 January 2021, the government published the results of its 2020 Housing Delivery Test, which aims to measure how effectively each local authority is delivering housing against NPPF requirement to demonstrate a five-year supply of deliverable housing sites plus five per cent land supply buffer as standard. Bristol was found to be delivering only 72% of the housing requirement. The penalties for this will be that Bristol will have to provide a "buffer" of sites for 20% more homes than are needed to meet their five-year target, will be required to produce a Housing Action Plan (which has been produced), and the presumption in favour of development in the NPPF will apply.

In view of the fact that the LPA is not able to demonstrate a five-year housing land supply, the current policies are deemed out of date, and paragraph 11(d) of the NPPF, and the 'tilted balance' is engaged.

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As set out under Key Issue (A) above, applying the ‘tilted balance’ to this application involves two aspects to understanding whether planning permission should be granted, which (as taken from NPPF paragraph 11 (d)) in this case can be summarised as:

- i. Whether the application of policies in the NPPF that protect Conservation Areas provide a clear reason for refusing the proposal;
- ii. Whether any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

In relation to point i. it has been identified that the only asset of relevance in this case would be the Gloucester Road Conservation Area. As covered below by Key Issue (G) *Urban Design and Heritage* below, the proposal is deemed to preserve and enhance the character of the Conservation Area and therefore meets the test of point i.

In relation to point ii. the question for this application is therefore, would any adverse impacts of granting planning permission significantly and demonstrably outweigh the benefits of providing housing? These matters are covered in detail below.

Local planning policy relating to housing proposals can be summarised as follows. Policy BCS5 ‘Housing Provision’ sets out the Core Strategy’s aim ‘...to deliver new homes within the built up area to contribute towards accommodating a growing number of people and households in the city’, and highlights that the ‘...minimum target will be 26,400 homes between 2006 and 2026’. Further, policy BCS5 identifies that the ‘...development of new homes will primarily be on previously developed sites across the city’. The development would contribute to the minimum new homes target discussed in policy BCS5 and would provide housing in a built-up area, as envisaged by the policy.

Policy BCS20 ‘Effective and Efficient Use of Land’ seeks to ensure that all developments maximise the use of previously developed land. The key expectation of the policy is that development uses land efficiently, achieving densities appropriate for the respective site. The policy expects appropriate densities for sites to be informed by the characteristics of the site, the local context, the site’s accessibility, the opportunities for a mix of uses across the site, the need to provide an appropriate mix of housing to meet the community’s needs and demands, and the need to achieve high-quality, well-designed environments.

Policy BCS10 (Transport and Access Improvements) of the Core Strategy states that development proposals should be located where sustainable travel patterns can be achieved, with more intensive, higher density mixed use development at accessible centres and along or close to main public transport routes. Proposals should minimise the need to travel, especially by private car, and maximise opportunities for the use of walking, cycling and public transport.

In common with policy BCS5 and BCS20, the NPPF also promotes the effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions (paragraph 119). Further, paragraph 120d of the NPPF expects planning decisions to amongst other things, ‘promote and support the development of under-utilised land and buildings, especially if this would help to meet identified needs for housing where land supply is constrained, and available sites could be used more effectively...’.

To summarise, as a proposal for new homes; the presumption in favour of sustainable development and ‘tilted balance’ is applicable in the light of the Housing Delivery Test results and the current absence of a five-year supply.



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To justify a refusal of planning permission, it would be necessary to demonstrate that any adverse impacts of granting permission would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

Redevelopment of the site for residential use to include a mix of dwellings and retail use is consistent with the Local Plan, the direction of emerging policy in the Local Plan Review March 2019 and the NPPF. The site is within a highly sustainable location close to local facilities and good public transport links adjacent to an existing residential area and is therefore an appropriate location for residential development. The principle of the residential-led redevelopment of the site would therefore be acceptable.

Together with this Key Issue, the remaining report assesses the development against the development plan, along with other material considerations, including the NPPF, culminating in a consideration of the planning balance, where any adverse impacts of the granting planning permission are weighed against its benefits, when assessed against the policies in the NPPF when taken as a whole.

*Loss of retail floorspace*

The existing site comprises over 800sqm of retail and ancillary space. Approved application 20/00022/F (extant until 15.02.24) includes an extension of the existing retail use into the rear yard of the site creating a larger retail space on ground floor (retail area – 645sqm, office – 42sqm) and retaining the retail area on the first floor (260sqm). On the second floor, additional office space is also provided (approx. 75sqm). This equates to an overall increase in retail area of 265sqm and a total of over 900sqm.

The current application proposes to retain 254sqm of retail space at ground floor to the Gloucester Road frontage with 1192.4sqm of residential use. The proposal relocates the main entrance to the retail area onto Gloucester Road to enhance the businesses presence in the street and locality.

Section 7 of the NPPF (Ensuring the vitality of town centres) states planning policies and decisions should support the role that town centres play at the heart of local communities, by taking a positive approach to their growth, management and adaptation.

The site is located within the designated Gloucester Road Town Centre as identified on the Site Allocations and Development Management Policies Local Plan (SADMP) Policies Map but is outside of the Primary Shopping Area and Secondary Shopping Frontages (Policy DM8 of the Bristol Site Allocations and Development Management Policies applies to these designations).

Policy BCS7 of the Bristol Core Strategy states that uses which contribute to maintaining the vitality, viability and diversity of centres will be encouraged. Active ground floor uses will be maintained and enhanced throughout the centres. Retail shop uses will predominate in the designated primary shopping areas of the City and Town Centres, supported by a wider range of appropriate uses in the other parts of these centres.

The loss of retail floor area in this location would be acceptable given that the site lies outside primary shopping area and secondary shopping frontage. In addition, the remaining retail floor area would be enhanced with new entrances onto Gloucester Road thereby contributing to an active frontage and the vitality of the area.

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Policy BCS20 (Effective and Efficient Use of Land) states that new development will maximise opportunities to re-use previously developed land. Where development is planned opportunities will be sought to use land more efficiently across the city. Imaginative design solutions will be encouraged at all sites to ensure optimum efficiency in the use of land is achieved. Higher densities of development will be sought:

- i. In and around the city centre;
- ii. In or close to other centres;
- iii. Along or close to main public transport routes.

For residential development a minimum indicative net density of 50 dwellings per hectare will be sought. Net densities below 50 dwelling per hectare should only occur where it is essential to safeguard the special interest and character of the area.

The Urban Living SPD (2018) outlines the Council's approach to delivering residential development of a high quality at higher densities. This document indicates that densities of 120units per hectare will be targeted within urban settings within the 'Inner Urban Area' such as this site. The Urban Living SPD (UL SPD) states: "The Inner Urban Area broadly comprises the city's Georgian and Victorian neighbourhoods, most of which fall within a 20-minute walk of the city centre in either a northerly, easterly or southerly direction. (Figure 2 map of the UL SPD refers).

Policy encourages effective and efficient use of land, particularly in maximising opportunities to re-use previously developed land within designated centres and along or close to major transport corridors. As discussed above, the site is located within the designated Gloucester Road Town Centre and benefits from very good public transport accessibility. The site is therefore well suited to accommodating higher density forms of residential development. Residential development at this location would encourage future occupants to primarily walk, cycle and use public transport, rather than owning a private car. More effective and efficient use of underutilised land in this location to deliver housing to meet local needs is supported and encouraged by policy. Residential development of this site to achieve higher densities is therefore supported.

The Urban Living Supplementary Planning Document (UL SPD) states that "Whilst ultimately it's the design outcome that is key, rather than the density figure, understanding density levels is useful. An unusually high or low density for the location should suggest further consideration of the brief and the aim of the scheme, together with additional scrutiny of elements that are made more complex by higher density."

Using the UL SPD calculation method for density, the scheme has been calculated as a density of 95 dwellings per hectare (dph). This would meet the minimum indicative net density of 50 dwellings per hectare specified by Policy BCS20 and would be similar to the density of 120dph targeted for the 'Inner Urban Area'. As set out within the supporting text to Policy BCS20, this is similar to streets of terraced housing such as in Southville (85dph) or Totterdown (120dph). The approach to density is therefore considered to be policy compliant and in keeping with the transitional character of the area and adjacency to the more densely developed Gloucester Road, with its commercial character.

**(D) HOUSING MIX AND BALANCE**

Policy BCS18 (Housing Type) of the Core Strategy states that all new residential development should maintain, provide or contribute to a mix of housing tenures, types and sizes to help support the creation of mixed, balanced and inclusive communities.

To achieve an appropriate tenure, type and size mix the development should aim to:

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- Address affordable housing need and housing demand;
- Contribute to the diversity of housing in the local area and help to redress any housing imbalance that exists;
- Respond to the requirements of a changing population;
- Employ imaginative design solutions.

2021 Census data can be a useful indicator of existing housing mix and balance. The relevant indicators are summarised below:

Redland ward

- 41% living in flats, maisonettes or apartments (35% for Bristol, 22% England and Wales).
- 40% of households have 4 or more bedrooms, 24% 3 bedrooms, 22% 2 bedrooms, 15% 1-bedroom.
- 26% one person households.
- Household ownership- 65% (outright or with mortgage), shared ownership- 0.3%, social rented- 3%, private rented- 31%

Berkeley Road South Lower Super Output Area (LSOA) (neighbourhood level)

- 28% living in flats, maisonettes or apartments (35% for Bristol, 22% England and Wales), 71% in houses.
- 44% of households have 4 or more bedrooms, 28% 3 bedrooms, 16% 2 bedrooms, 12% 1-bedroom.
- 25% one-person households, 31% two-person households, 17% 3-person households, 17% 4-person households, 6% 5-person households, 2.5% 6 or more person households.
- Household ownership- 67% (outright or with mortgage), shared ownership- 0.2%, social rented- 2%, private rented- 31%

The proposal is for flats and given that the predominant housing type in the ward and LSOA is houses, this would contribute to the housing diversity of the area. The housing mix in terms of sizes would also be acceptable in light of the data- see summary below- and would help to increase the availability of smaller properties.

The proposed residential development in this instance would comprise a flatted scheme including a total of 17 flats comprising:

- 1 bedroom x 7 (1 bedroom 2 person x 7)
- 2 bedroom x 7 (2b3p x 3 2b4p x 4)
- 3 bedroom x 3 (3b6p x 1 and 3b4p x 2 which would be only 3sqm below the space standard for a 3b5p home).

Overall, the development proposes to take an inefficiently managed and underutilised town centre retail site and develop it to deliver an appropriate mix of retail and residential use and more efficient use of previously developed land in a sustainable location. This would have both social, economic and environmental benefits.

**(E) AFFORDABLE HOUSING PROVISION**

The Council's planning policies for affordable housing in Bristol are set out in Policy BCS17: Affordable Housing Provision in the Core Strategy Local Plan (Adopted June 2011), and Policy DM3: Affordable Housing Provision: Smaller Sites in the Site Allocations and Development Management Policies Local Plan (Adopted July 2014). Further guidance on the Council's affordable housing policies is set out in the Affordable Housing Practice Note 2022 (AHPN).

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The site falls within Redland ward, which is in Inner West Bristol. In accordance with policy BCS17 the site is required to deliver 40% affordable housing, which is 6.8 units out of the 17 homes being delivered.

The application submission stated that the provision of affordable housing would be unviable. Following submission of a viability assessment and its review, it has been established that affordable housing can be provided, albeit not at the level or tenure specified in the policy (40%).

The Council's Housing Enabling Manager has indicated that given the small number of properties comprised in the development, and the fact that housing associations are unlikely to be interested in taking only a few units within a predominantly open market block, they would not require the provision of social rent or shared ownership on-site affordable housing. Instead, they advised that the Council accept the provision of First Homes, sold at a 30% discount to open market value.

It has been agreed by the applicant that 4 First Homes be included within the scheme, sold at 30% discount to open market value. In order to deliver affordable homes on site it has been agreed that 4 no. 1b2p be provided as First Homes. These are flat numbers 3, 4, 7 and 12 as shown on plan and would be secured through section 106 planning agreement in accordance with the relevant terms.

**(F) TRANSPORT AND HIGHWAY MATTERS**

The National Planning Policy Framework (NPPF) paragraph 104 (in summary) seeks that development take account of:

- Impact on transport networks;
- Opportunities from existing/ proposed infrastructure;
- Promotion of walking/ cycling/ public transport;
- Environmental impact and mitigation;
- Patterns of movement, streets and parking to contribute to high-quality places.

Paragraph 105 of the NPPF states that "Significant development should be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes."

NPPF Paragraph 111 states that "Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe."

Para. 112 states that applications should (summarised):

- (a) Give priority to pedestrian and cycle movements;
- (b) Address the needs of people with disabilities/ reduced mobility in relation to all modes of transport;
- (c) Create safe, secure, attractive places- which minimise conflict between users, avoid unnecessary street clutter, and respond to local character and design standards
- (d) Allow for the efficient delivery of goods, and access by service and emergency vehicles;
- (e) Be designed to enable charging of plug-in and other ultra-low emissions vehicles in safe, accessible and convenient locations.

Policy BCS10 (Transport and Access Improvements) of the Core Strategy states that development proposals should be located where sustainable travel patterns can be achieved, with more intensive higher density mixed use development at accessible centres and along or close to main

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public transport routes. Proposals should minimise the need to travel, especially by private car, and maximise opportunities for the use of walking cycling and public transport.

It goes on to state that developments should be designed and located to ensure the provision of safe streets and reduce as far as possible the negative impacts of vehicles such as excessive volumes, fumes and noise. Proposals should create places and streets where traffic and other activities are integrated and where buildings, spaces and the needs of people shape the area.

The following hierarchy for transport user priorities is set out:

- a) The pedestrian;
- b) The cyclist;
- c) Public transport;
- d) Access for commercial vehicles;
- e) Short stay visitors by car;
- f) The private car.

Policy DM23 (Transport Development Management) of the SADMP outlines that new development should not give rise to unacceptable traffic conditions and will be expected to provide safe access to the highway network. The policy also outlines that new development should be accessible by sustainable transport methods such as walking, cycling and public transport. Furthermore, the policy sets standards for parking provision. These can be found at Appendix 2 of the SADMP document. The parking standards are maximum levels for car parking and minimum levels for cycle parking.

Policy DM32 (Recycling & Refuse Provision in New Development) of the SADMP outlines that all new development should provide bin and recycling storage facilities fit for the nature of development, with adequate capacity for the proposed development, in a location which is safe and accessible for all users and does not harm the visual amenity of the area or neighbouring amenity.

The site is located to the western side of Gloucester Road within Redland ward. This is approximately 1.4km north of the city centre boundary. Gloucester Road (A38) is a major arterial route providing access to the city from the northern suburbs and beyond. Gloucester Road is served by a range of bus services. There is a north and outbound stop outside Bishopston Library immediately south of the site. South and inbound services are accessible by stops 200m from the site. The site is also around 1km from Redland or Montpelier Rail Stations which provide a further public transport option to access neighbouring suburbs, the city centre and the wider national rail network. The site is also located within a designated town centre which includes a range of shops, services and facilities. Given the proximity, it would be convenient for residents to shop and make use of local services and facilities on foot. The site would represent a sustainable and policy compliant location for the development proposed.

The application proposal is for 3 car parking spaces to serve the retail units(s) with 1 disabled car parking bay to serve the residential flats (as required by policy). 26 secure and covered cycle parking spaces are proposed within a bike store to the rear of the property.

Local policy standards relating to car parking levels are maximum levels and there is no minimum residential parking requirement. Minimum thresholds are however specified for cycle parking and disabled parking. The standard for the cycle parking is 27 spaces, however the proposed cycle store appears able to accommodate a further cycle stand, and this can be secured by condition.

Provision of a car club parking space would not be sought for a development of this size, nor would a dedicated servicing bay as there is no policy standard requiring this. The nearest existing car club space is shown (Travelwest) to be approximately 300m way on Effingham Road.

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The existing arrangement has 10 off-street vehicle parking spaces serving the current retail unit. Planning permission 20/00022/F includes 8 no. parking spaces on the site to serve the retail use, with no on-site car parking provision for the residential development (9 flats).

Objection has been received from local residents/ interested parties regarding the quantum of residential development and lack of parking provision for the residential units, with existing parking and traffic pressures and associated highway safety concerns cited. Refer to 'Response to Publicity and Consultation' section for full details.

It is acknowledged that this is an area of traffic and parking pressure, particularly given that the site is not within a Residents Parking Scheme (RPS) but is located close to existing RPS areas (the site is approximately 0.5km north of the nearest RPS boundary). Such areas can be a focus for commuter parking.

2021 Census data for car ownership levels for flats within this Lower Super Output Area (the Berkeley Road South neighbourhood area), show 53% of existing households have 1 car, 8 % have two cars and 2% with 3 or more cars. For this development proposal, this would equate to approximately 11 cars total.

The extant permission allows for 9 new residential units without off-street parking (an estimated car ownership level of approximately 5 cars) so the assessment should be made based on the additional 8 residential units proposed over that level which would be an additional 6 cars requiring on-street parking. These are estimated levels based on existing Census data and for a site in such a sustainable location and for a car-free development, this level would be expected to be lower. Changes in working patterns particularly since the pandemic have also shown increases in people working from home, which may affect car ownership levels. The 2021 Census data for this ward (noting the likely impact of the pandemic on this data) showed 55% working mainly from home, 3% via bus/ minibus/ coach, 20% driving and 20% via 'active travel' modes.

The proposal would lead to the creation of on-street parking of 2-3 car parking spaces, where the existing off-street parking would be removed. A tracking diagram for a fuel tanker to enter the adjacent petrol filling station has demonstrated the extent of on-street parking space creation possible. A Traffic Regulation Order (TRO) for parking restrictions along the site's frontage would be required (at a cost of £6,310 to be secured via section 106 agreement).

In regard to highway safety, local junctions currently have parking restrictions maintaining safety at these points and TDM advises that the additional car parking demand would not be concluded to result in an unacceptable impact upon the highway or to result in a highway safety issue. An advice note would be recommended to alert future purchasers that should the site become part of a future RPS, that as a car free development, they may not be eligible for a parking permit.

The Council's Transport Development Management Team (TDM) advises that they "acknowledge the constrained parking in this location. The site is considered to be situated in a sustainable location given the range of bus services in close vicinity, range of local amenities and ability to cycle and walk to a number of destinations in a short period of time. On this basis and given the local and national policy in favour of supporting development in sustainable locations to reduce reliance on the private car TDM are content and raise no concern regarding the lack of parking for residents."

The applicant would be required to carry out the following highway works to be secured via condition and section 278 agreement post-planning:

- Reconstruct the footway along the length of the site;
- Install pennant kerbing along the site frontage;
- Upgrade or install street lighting;

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- Remove the small existing street tree on Berkeley Road and replace with a new tree and tree pit;

Bristol Waste has reviewed the proposed plans and confirms the acceptability of the proposed access arrangements. The exact proposed waste bin provision would be specified by Bristol Waste prior to occupation however the plans demonstrate that there is sufficient space for the necessary waste storage. A condition to secure final details is recommended.

A Construction Environmental Management Plan (CEMP) would be a condition requirement should permission be granted. Please refer to the proposed conditions list at the end of this report for details of all highway conditions recommended.

**(G) URBAN DESIGN AND HERITAGE**

The NPPF was revised in June 2021 to strengthen the requirement for good design. Paragraph 134 states:

Development that is not well designed should be refused, especially where it fails to reflect local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes. Conversely, significant weight should be given to:

- a) development which reflects local design policies and government guidance on design, taking into account any local design guidance and supplementary planning documents such as design guides and codes; and/or
- b) outstanding or innovative designs which promote high levels of sustainability, or help raise the standard of design more generally in an area, so long as they fit in with the overall form and layout of their surroundings

New development is expected to establish a strong sense of place, and to be visually attractive as a result of good architecture, being sympathetic to the local character and history, while not preventing appropriate change (including increased densities).

In addition, requiring good design is at the heart of Bristol planning policy, and BCS21 expects a high quality design in all developments, which contributes positively to an area's character and identity, creating or reinforcing local distinctiveness. Policy DM26 requires new development to respond positively to the site, creating and enhancing public spaces and responding appropriately to height scale and massing of existing buildings. Policy DM27 requires development to achieve a coherent, interconnected and integrated built form, and to use trees and plants appropriate to the character of the area. Policy DM28 requires development to incorporate high quality and inclusive public realm, which is well surveilled and reduces crime and the fear of crime. Policy DM29 requires all new buildings to respond to their solar orientation, incorporate active frontages and clearly defined main entrances facing the public realm that emphasise corners and reinforce the most prominent frontages.

The Council's Urban Living SPD as well as the Development Brief both recognise the need to deliver at least 33,500 new homes by 2036. Urban Living SPD seeks to guide development towards creating compact, characterful and healthy urban areas, and to ensure that development contribute to healthy places and living environments for future residents.

The application proposal is largely the same design as the extant permission 20/00022/F, with the exception of additional dormer windows to the Gloucester Road elevation of the former chapel roof and additional landscaping and boundary treatments to Berkeley Road where off-street parking

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would be replaced by landscaped areas. The new-build element would have the same 'envelope' as that already approved.

Nonetheless, the Local Planning Authority is required (under Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990) to pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area. Where there is harm to a listed building or a conservation area the decision maker must give that harm considerable importance and weight.

The site is within the Gloucester Road Conservation Area (a designated heritage asset) and the Former Methodist Chapel is a locally listed building (a non-designated heritage asset).

Section 16 of the NPPF sets out the expectations for the role planning decisions should play in conserving and enhancing the historic environment. Where a development proposal would lead to less than substantial harm to the significance of a designated heritage asset (including a conservation area), this harm should be weighed against the public benefit of the proposal, including securing its optimum viable use (para. 202). The impact of a proposal on a non-designated heritage asset (the locally listed building) requires a balanced judgement having regard to the scale of any harm or loss of the significance of that asset (para. 203).

In addition, policy BCS22 states that: *“Development will safeguard or enhance heritage assets and the character and setting of areas of acknowledged importance including historic buildings both nationally and locally listed... and conservation areas.”* Policy DM31 requires that *“proposals affecting locally important heritage assets should ensure they are conserved having regard to their significance and the degree of harm or loss of significance”*.

Officers are satisfied that there would be no greater heritage impact than the previously consented scheme 20/00022/F and in fact there would be some enhancements through improved landscaping and street scene impact. The proposed dormer windows to be introduced on the north roof slope of the former chapel do have a degree of impact on the architectural character of the historic building. The impact is very low and proportionate to bringing the building into a new viable use.

The Council's Conservation Officer has advised that they are satisfied regarding the heritage impact of the proposals. This takes into account (and gives great weight to elements of harm) the proposed part demolition of the locally listed building (which is kept to a minimum), demolition of the modern element that has a negative impact on the chapel building and conservation area, the overall enhancement of the chapel building proposed and other site enhancements including the public benefits of contribution to housing supply and street scene enhancement.

The Council's Urban Design Officer has raised concerns about several aspects of the development in terms of urban living considerations (as set out within the Urban Living Supplementary Planning Document- SPD). These aspects include the long circulation corridors without natural light and ventilation and the proportion of single aspect units. However, on balance they support the application overall subject to conditions to secure detailed design elements including methods of security and reducing opportunities for crime. A condition is also recommended seeking confirmation that there is a contract in place to deliver full fibre broadband to the development.

#### (H) HEALTH AND SAFETY EXECUTIVE (FIRE SAFETY RISK)

The Health and Safety Executive Planning Gateway One system, set up following the Grenfell Tower tragedy, considers the fire safety risk of certain categories of new buildings through the planning process. This process applies to all applications registered after 1 Aug 2021. In this case, the application was registered prior to that date in July 2021. However, it is unlikely that this process



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would apply to this development in any event as it applies to buildings where the top-floor floor level is 18m or over and in this case, this measurement would be approximately 6.3m.

**(I) RESIDENTIAL AMENITY- NEIGHBOURING AND FUTURE OCCUPIERS**

This Key Issue considers the proposal's impact on neighbouring properties and future occupiers in accordance with development plan policies, which require consideration to matters of privacy, outlook, natural lighting, ventilation, and indoor and outdoor space (see policies BCS20, BCS21, DM27, and DM29). These policy expectations concerning existing neighbour's amenity are consistent with the NPPF, for example paragraph 130 of the NPPF which expects planning decisions to ensure new developments create places with a high standard of amenity for existing and future users. The Development plan policy and the Urban Living Supplementary Planning Document (UL SPD), both seek to ensure that existing and future occupiers are not prejudiced, while encouraging developments to make an efficient use of land.

*Neighbouring occupiers*

With the above in mind, it is important to ensure the application of daylight and sunlight assessments does not prejudice the development's ability to make an efficient use of the site to deliver housing, whilst also ensuring the development does not result in unreasonable impacts on neighbours, including unacceptable living standards for residential uses. It is therefore material to acknowledge that the site is very constrained in terms of where residential development can be successfully located.

Although this proposal involves an increase in the number of residential units proposed, this is achieved based on the same building envelope (the ground floor footprint has reduced compared to permission 20/00022/F and the upper floor footprints remains the same).

The key consideration is the relationship of the south elevation to neighbouring properties adjacent at 4 Berkeley Road and properties to the south within the library development, which have windows and terraces overlooking and immediately adjoining the application site. The proposal would maintain the same separation distance as the approved scheme and while two additional window openings are proposed these are minimal and the number of balconies compared to the approved scheme is also reduced. The proposed second floor balconies originally proposed by the current application have now been removed from the scheme. There would also be new ground floor openings created compared to the approved development, but these would be lower than adjacent terraces and windows so would not lead to overlooking. Opaque glazed privacy screens, 1.8m in height from roof level are proposed to the sides of the terraces to prevent direct overlooking towards neighbouring properties. These will be secured by condition.

In conclusion, whilst there are some tight relationships which result surrounding the site, these do not cause significant detriment to the overall quality of amenity at any adjacent property. Given the increased density which is encouraged by policy, some reduction in spaciousness is inevitable. The applicant has made amendments to the scheme in this instance to address areas of particular concern. Following these, it is concluded that the development would preserve an acceptable standard of amenity for all neighbouring occupiers in accordance with the requirements of national and local planning policy.

*Future residents*

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Policy DM29 (Design of New Buildings) of the SADMP states the design of new buildings should be of high quality. To achieve this, new buildings are expected to ensure that existing and proposed development achieves appropriate levels of privacy, outlook and daylight. New residential development should provide dual aspect where possible, particularly where one of the aspects is north-facing.

Within the proposed development, 13 units would be single aspect (76%). Of these, 8 flats would be south or east-facing and 5 flats would also have access to a small garden or balcony, improving the overall living environment.

The Urban Living SPD sets out recommended private amenity space provision based on 5sqm for a 1-2 person home and an additional 1sqm per additional occupant thereafter. Based on the proposal, this would be a total amenity space requirement of 104sqm. 7 of the homes would have external space that would meet the individual recommendation or nearly meet it – in the case of Flats 1 and 3, the balconies would be slightly below the space sought. The recommended amenity space calculation for the remaining flats (those without gardens or balconies) would be calculated to be 64sqm. The communal garden (measuring the main, useable space) would measure 67sqm in area, and though quite a constrained space, overall the proposed development would provide the recommended level of external amenity space albeit for the majority of units, this would be in the form of communal space. A landscaping condition is recommended relating to the detail of this space.

The SPD states that “Where sufficient private open space cannot be accommodated on site, due to identified constraints, proximity to existing open space may be considered.” The SPD also seeks provision for children’s playspace and provides a calculation for developments. This proposal has insufficient communal space to include specific children’s playspace, though one of the larger units would benefit from private external space of almost 60sqm. Furthermore, St Andrew’s Park is approximately 350m walk away (500m to the children’s play area).

Taking into account the particular constraints of the site and historic building as well as the wider benefits of the proposals, officers recommend that this represents an acceptable arrangement and approach to this particular site. The overall level of amenity provided to future residents would be acceptable. While the provision of more dedicated external space (private or communal) would be welcomed, it is recognised that this is difficult to achieve within the constraints of the site and given proximity to the amenities of Gloucester Road and the nearby St Andrews Park, this is acceptable.

The relevant space standards are the Ministry of Housing, Communities & Local Government, Technical housing standards - nationally described space standard (2015). These outline technical standards for application to all tenures of new housing across England. The standards set minimum internal areas which accommodation should provide relative to the number of future occupants. The development has been assessed against the standards and is compliant in all respects. This will ensure that the development delivers homes which offer sufficient space to accommodate the everyday living and needs of future occupants.

## (J) SUSTAINABILITY

As embedded in the NPPF, sustainability should be integral to all new development, and should encourage opportunities for development to draw its energy supply from decentralised, renewable Item no. 1 Development Control Committee A – 24 August 2022 Application No. 21/06878/F : Land At Corner Of York Road And St Luke’s Road Bedminster Bristol BS3 4AD or low carbon energy supply systems. Core Strategy Policy BCS13 encourages developments to respond pro-actively to climate change, by incorporating measures to mitigate and adapt to it. BCS14 sets out a heat hierarchy for new development, and an expectation that new development will connect to existing

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CHP/CCHP distribution networks. The same policy also expects development to provide sufficient renewable energy generation to reduce carbon dioxide emissions from residual energy use in the buildings by at least 20%. BCS15 requires developments to demonstrate through a Sustainability Statement how they have addressed energy efficiency; waste and recycling; conserving water; materials; facilitating future refurbishment and enhancement of biodiversity.

The submitted Sustainability and Energy Statement updated during the course of the application has been assessed by the Sustainable City Team as being acceptable subject to conditions.

The key features of the proposed development in terms of addressing the sustainability policies include the provision of a communal air source heat pump system (ASHP) within the external space to the rear of the property. In addition, a solar photovoltaic array is proposed to the roof pitches. A condition to secure details of the noise output levels of the ASHPs is recommended, along with any noise mitigation that may be necessary.

An Overheating Assessment has been submitted for the units based on thermal modelling undertaken of the proposal. This is based on mechanical ventilation and heat recovery (MVHR) with built-in cooling and integral blinds across all units.

Such assessments are required to model for present-day temperatures but also expected future temperatures using predicted weather modelling for the years 2050 and 2080 (taking into account the design life of developments). In this case, the overheating assessment demonstrates that the proposal would meet requirements for the present-day but would require MVHR and external louvres or cooling to meet the 2050 and 2080 temperatures.

Further negotiation took place regarding the proposed external louvres (which allow windows at ground floor to be open at night while providing security and allow windows at other floors to be open at night while providing noise mitigation). It was advised that the external louvres proposed raised concerns in terms of visual impact (especially in terms of the former chapel) and amenity as they would prevent a view out of many rooms.

The subsequent Overheating Assessment then omitted these features and proposes MVHR, cooling (mechanical) and integral blinds to meet the overheating criteria for all units for the years 2050 and 2080. However, cooling is not policy compliant since it increases energy demand and associated CO<sub>2</sub> emissions so alternative approaches would be advised. The use of blinds to mitigate overheating is not supported as it relies on occupier intervention and can affect liveability if required to be closed for long periods.

In some cases where the mitigation measure is only required to pass criteria under the 2080 weather file and would require replacement prior to then anyway, it may be appropriate to identify this as a suitable retrofit measure whilst demonstrating that the building has been designed in such a way to accommodate it in the future. However, in this case mitigation is required to meet both the 2050 and 2080 weather files.

Alternative suggested approaches to addressing this issue include increasing natural ventilation (window openings/ ventilation louvres/ increasing thermal mass) or reducing the g-values of the windows to reduce solar gain. If these measures would be insufficient, additional measures including louvres/ external shading/ reducing the area of glazing may be acceptable. There may be alternative louvre/ shading options that would be acceptable to some residential units subject to detailed design considerations or other alternatives.

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The applicant was asked to respond to these alternatives and a summary and explanation of the key considerations is set out below:

- Building Regulations requires ground floor windows to be closed at night and window restrictors, which limit the amount of natural ventilation achievable;
- For ground floor rooms, agreement for windows to be open at night perhaps through some other security measures could be an option, external ventilation louvres with a more acceptable appearance could be explored or mechanical cooling (as proposed).
- Ventilation louvres (which allow ventilation while providing security as required to ground floor rooms or noise mitigation) would have a visual impact on the proposal. External louvres/shading would be unacceptable to the former chapel building, a heritage asset. While external louvres may be acceptable in visual terms to the new-build element and rear elevation if appropriately designed, those explored to date were unacceptable in visual terms (projecting 20cm from the window face and obscuring a view out of the windows of many rooms).
- By 2080, 2 complete service refits would be anticipated, with associated improvements in thermal efficiency and cooling benefits so these measures can be retrofitted later.

In summary, the matters have not been fully addressed to the satisfaction of the Sustainable City Team. However, it is clear that there are a number of constraints in terms of considerations for the site. Should Members be minded to recommend approval, it is advised that a suitable worded condition be attached requiring the various options to be explored further – please see recommended conditions list for appropriate wording. It should be noted that external alterations required may require further permission, however it may be possible to secure such alterations as amendments to any permission granted.

**(K) TREES, LANDSCAPING AND NATURE CONSERVATION**

Immediately outside the site on Berkeley Road is located a small street tree that has been replaced but is not thriving and has been damaged, presumably by vehicles. This street tree should be replaced with a new specimen and can be secured through the section 278 process.

A Biodiversity Net Gain Assessment is not an application requirement in this particular case due to the date of application submission (July 2021). However, based on the proposals it can be demonstrated that there would be a biodiversity net gain contained to the existing situation through provision of landscaping and tree planting. Relevant conditions would be required to secure the proposed landscaping and tree planting.

A bat and protected species has been carried out and is up-to-date and would be the subject of a planning condition should approval be granted (see recommended conditions).

**(L) NOISE ASSESSMENT AND POLLUTION CONTROL**

An Acoustic Report supports the application relating to the impact of traffic noise on future residents of the site. The Council's Pollution Control Officer raises no objection on this basis.

They recommend a planning condition relating to noise insulation between the ground floor commercial use and first floor residential use alongside a number of other general conditions- see full list below. A condition is also required to secure details of the proposed air source heat pumps in order to ensure that products with an appropriate noise level are selected.

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**(M) FLOOD RISK AND SUSTAINABLE DRAINAGE**

The site is in Flood Zone 1 and there are no objections on flood risk grounds. A condition is recommended in respect of sustainable drainage system (SuDS) requirements.

**(N) CONTAMINATED LAND**

The revised geotechnical report has been considered. The principal difference between this scheme with respect to the risks from contamination and that already permitted is the inclusion of soft landscaping areas to the rear of the premises. The proposed mitigation is acceptable, therefore if approved, a number of conditions are recommended.

**(O) AIR QUALITY**

The Council's Air Quality Officer has advised that monitoring data shows that compliance with objectives will be achieved in this location and therefore does not object to the application.

**(P) PLANNING OBLIGATIONS**

Planning obligations must accord with section 122 of the Community Infrastructure Levy Regulations 2010 and paragraphs 55 and 57 of the NPPF, in that a planning obligation may only constitute a reason for granting planning permission for a development if the obligation is:

- necessary to make the development acceptable in planning terms;
- directly related to the development; and
- fairly and reasonably related in scale and kind to the development.

The following planning obligations would be required by this development proposal:

- i. Affordable housing- provision of 4 no. First Homes.
- ii. Traffic Regulation Order contribution for the provision of parking restriction outside the site- to be confirmed (sum of £6310.00)

Should Members determine to approve the application, officers would seek delegated authority to complete the section 106 agreement for the above obligations.

**(Q) BENEFITS AND PLANNING BALANCE**

Officers acknowledge the Government's 2020 Housing Delivery Test (HDT) results that indicate that the Council's delivery of housing was below (less than 75%) the housing requirement over the previous 3 years. Further, the Council currently cannot demonstrate a deliverable 5 year housing land supply. This means that the 'tilted balance' set out in NPPF paragraph 11(d)(ii) applies. Specifically, paragraph 11 makes it clear that plans and decision should apply a presumption in favour of sustainable development, with section (c) of this paragraph explaining that development proposals that accord with an up-to-date development plan should be approved without delay. However, section (d) goes on to explain that where there are no relevant development plan policies, or where the policies which are most important for determining the application are out-of-date, planning permission should be granted unless:

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- i. the application of policies in the NPPF that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or
- ii. any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this NPPF taken as a whole.

There are a number of conflicts with the development plan specifically in terms of the number of single aspect units proposed and overheating considerations (Policies DM27, DM29, BCS13, BCS14 and BCS15). However, the overheating consideration can be dealt with via appropriate condition and the higher proportion of single aspect units is deemed to be off- set by the external amenity space and south-facing aspect of some of these units.

The fact that policies have to be considered out-of-date does not mean that they can carry no weight. To carry weight, policies must be consistent with the NPPF, as explained in its paragraph 213 which, amongst other things, explains that the closer the policies in the plan are to the policies in the NPPF, the greater the weight that may be given to them. As such, it is perfectly possible for policies which are deemed out-of-date for reasons of an inadequate housing land supply to still carry significant weight.

In this case, officers consider that to be the case here, as all the policies cited within this report for reasons to refuse the development are consistent with the NPPF. The policies referenced should therefore still all carry significant weight in the determination of this application. No policies covered by NPPF paragraph 11(d)(i) apply in this case, so the application should be determined in the context of NPPF paragraph 11(d)(ii).

Benefits would flow from approving this development, and these are acknowledged. The proposal would offer a contribution to housing supply, including affordable homes for which the presumption in favour of sustainable development and the tilted balance apply. This benefit therefor carries substantial weight. There would be benefits in terms of the restoration of the existing locally listed building and site, bringing it back into active use and the efficient use of the land. The proposal would represent an enhancement to the character and appearance of the Conservation Area and would include green infrastructure and biodiversity benefits. The redevelopment has been designed to current sustainability standards and would therefore provide residential accommodation with both environmental benefits and benefits to future residents in terms of health and energy costs, as well as a good level of cycle parking provision and well-designed waste storage. There would be benefits in terms of employment during the construction period.

The benefits of the proposal would be deemed to outweigh the minor conflicts identified and accordingly, it is recommended that permission is granted (subject to planning agreement) without delay.

**(R) RECOMMENDATION**

That the Applicant be advised that the Local Planning Authority is disposed to grant planning permission, subject to the completion, within a period of six months from the date of this committee, or any other time as may be reasonably agreed with the Service Director, Planning and Sustainable Development and at the Applicant's expense, of a planning agreement made under the terms of Section 106 of the Town and Country Planning Act 1990 (as amended), entered into by the Applicant to cover the following matters:

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- i. Affordable housing- provision of 4 no. First Homes.
- ii. Traffic Regulation Order contribution for the provision of parking restriction outside the site- to be confirmed (sum of £6310.00)

**COMMUNITY INFRASTRUCTURE LEVY**

The CIL liability for this development is £87,829.43, however social housing relief may be claimed on those residential dwellings included in the development that are either (a) to be managed by a Housing Association for the provision of affordable housing, or (b) First Homes.

**RECOMMENDED** GRANT subject to Planning Agreement

**Condition(s)**

**Time limit for commencement of development**

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

**Pre commencement condition(s)**

2. Highway works - General arrangement plan

No development shall take place until general arrangement plan(s) to a scale of 1:200 showing the following works to the adopted highway has been submitted to and approved in writing by the Local Planning Authority.

Reconstruction of the footway along the length of the site

Where applicable indicating proposals for:

- o Existing levels of the finished highway tying into building threshold levels
- o Alterations to waiting restrictions or other Traffic Regulation Orders to enable the works
- o Signing, street furniture, street trees and pits
- o Structures on or adjacent to the highway
- o Extent of any stopping up, diversion or dedication of new highway (including all public rights of way shown on the definitive map and statement)

No development shall take place over the route of any public right of way prior to the confirmation of a Town & Country Planning Act 1990 path diversion/stopping up order.

Prior to occupation these works shall be completed to the satisfaction of the Highway Authority and approved in writing by the Local Planning Authority.

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Reason: In the interests of public safety and to ensure that all road works associated with the proposed development are: planned; approved in good time (including any statutory processes); undertaken to a standard approved by the Local Planning Authority and are completed before occupation.

3. Construction Environmental Management Plan - Major Development

No development shall take place, including any demolition works, until a construction management plan or construction method statement has been submitted to and approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the demolition/construction period. The plan/statement shall provide for:

1. A construction programme including phasing of works and construction methodology;
2. 24 hour emergency contact number;
3. Hours of operation (including deliveries and removal of plant, equipment, machinery and waste from the site) plus procedure for emergency deviation from permitted hours;
4. Expected number, type and size of vehicles accessing the site including cranes;
5. Details of management of deliveries, waste, equipment, plant, works, visitors- the use of a consolidation operation or scheme for the delivery of materials and goods;
6. On-site facilities (i.e. portacabins) and locations for storage of plant/waste/construction materials;
7. Routes for construction traffic, avoiding weight and size restrictions to reduce unsuitable traffic on residential roads;
8. Locations for loading/unloading, waiting/holding areas and means of communication for delivery vehicles if space is unavailable within or near the site;
9. Arrangements for the turning of vehicles, to be within the site unless completely unavoidable. Arrangements to receive abnormal loads or unusually large vehicles and swept paths showing access for the largest vehicles regularly accessing the site and measures to ensure adequate space is available;
10. Any necessary temporary traffic management measures;
11. Measures to protect vulnerable road users (cyclists and pedestrians);
12. Arrangements for temporary facilities for any bus stops or routes;
13. Method of preventing mud being carried onto the highway;
14. Means by which a reduction in the number of movements and parking on nearby streets can be achieved (including measures taken to ensure satisfactory access and movement for existing occupiers of neighbouring properties during construction);
15. Travel planning: car sharing, use of local workforce, parking facilities for staff and visitors, a scheme to encourage the use of public transport and cycling;
16. Methods of communicating the Construction Environmental Management Plan to staff, visitors and neighbouring residents and businesses and procedures for maintaining good public relations including complaint management, public consultation and liaison. Bristol City Council encourages all contractors to be 'Considerate Contractors' when working in the city by being aware of the needs of neighbours and the environment.
17. Arrangements for liaison with the Council's Pollution Control Team;
18. Mitigation measures as defined in BS 5528: Parts 1 and 2 : 2009 Noise and Vibration Control on Construction and Open Sites shall be used to minimise noise disturbance from construction works.
19. Control measures for dust and other air-borne pollutants. This must also take into account the need to protect any local resident who may have a particular susceptibility to air-borne pollutants.
20. Measures for controlling the use of site lighting whether required for safe working or for security purposes.



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Reason: In the interests of safe operation of the adopted highway in the lead into development both during the demolition and construction phase of the development and in the interests of the amenities of surrounding occupiers during the construction of the development.

4. Tree Protection and Arboricultural Method Statement

Prior to the commencement of the development hereby approved (including demolition and all preparatory work), a scheme for the protection of retained trees (including adjacent street trees), in accordance with BS5837:2012, including a tree protection plan (TPP) and an arboricultural method statement (AMS) shall be submitted to and approved in writing by the Local Planning Authority.

Specific issues to be dealt with in the TPP and AMS:

- a) Location and installation of services/ utilities/ drainage.
  - b) Methods of demolition within the root protection area (RPA as defined in BS5837: 2012) of the retained trees.
  - c) Details of construction within the RPA or that may impact on the retained trees.
  - d) A full specification for the installation of boundary treatment works.
  - e) A full specification for the construction of any roads, parking areas and driveways, including details of the no-dig specification and extent of the areas of the road, parking areas and driveways to be constructed using a no-dig specification. Details shall include relevant cross sections through them.
  - f) Detailed levels and cross-sections to show that the raised levels of surfacing, where the installation of a no-dig surfacing within Root Protection Areas is proposed, demonstrating that they can be accommodated where they meet with any adjacent building damp proof courses.
  - g) A specification for protective fencing to safeguard trees during both demolition and construction phases and a plan indicating the alignment of the protective fencing.
  - h) A specification for scaffolding and ground protection within tree protection zones.
  - i) Tree protection during construction on a TPP and construction activities clearly identified as a prohibited in this area.
  - j) Details of site access, temporary parking, on site welfare facilities, loading, unloading and storage of equipment, materials, fuels and waste as well as concrete mixing and use of fires.
  - k) Boundary treatments within the RPA.
  - l) Methodology and detailed assessment of root pruning.
  - m) Arboricultural supervision and inspection by a suitably qualified tree specialist.
  - n) Reporting of inspection and supervision.
  - o) Methods to improve the rooting environment for retained and proposed trees and landscaping.
  - p) Veteran and ancient tree protection and management.
- The development thereafter shall be implemented in strict accordance with approved details unless otherwise agreed in writing by the Local Planning Authority.

Reason: Required prior to commencement of development to satisfy the Local Planning Authority that the trees to be retained will not be damaged during demolition or construction and to protect and enhance the appearance and character of the site and locality, in accordance with DM17 and pursuant to section 197 of the Town and Country Planning Act

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1990.

5. Implementation/Installation of Retail Refuse Storage and Recycling Facilities - Shown on Approved Plans

Prior to the commencement of the development hereby approved, revised plans indicating a revised retail waste and recycling store to be located with external access and ventilation to the exterior has been submitted to and approved in writing by the Local Planning Authority.

No building or use hereby permitted shall be occupied or the use commenced until the retail unit refuse store and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans.

Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the adopted highway (including the footway), except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises; protect the general environment; prevent any obstruction to pedestrian movement and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

- 6 . Overheating Assessment

Prior to the commencement of the development hereby approved, a revised overheating assessment shall be submitted to and approved in writing by the Local Planning Authority to demonstrate alternative solutions to addressing identified overheating risks.

The development shall thereafter be carried out only in accordance with the approved details prior to the first occupation of the residential use and shall be maintained as such in perpetuity thereafter.

Reason: To ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions through reducing the need for cooling.

7. Details of Photovoltaics (PV)

1) Prior to commencement, details of the proposed PV system including location, dimensions, design/ technical specification together with calculation of annual energy generation (kWh/annum) and associated reduction in residual CO<sub>2</sub> emissions shall be provided within the Energy Statement.

2) Prior to occupation the following information shall be provided:

- Evidence of the PV system as installed including exact location, technical specification and projected annual energy yield (kWh/year) e.g. a copy of the MCS installer's certificate.
- A calculation showing that the projected annual yield of the installed system is sufficient to reduce residual CO<sub>2</sub> emissions by the percentage shown in the approved Energy Statement.

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Reason: To ensure that the development contributes to mitigating and adapting to climate change and to meeting targets to reduce carbon dioxide emissions.

## 8. Further details: Green roof

Prior to installation of the cycle store green roof hereby approved, a strategy for its implementation shall be submitted to and approved in writing by the Local Planning Authority. The strategy must include details relating to the extent, substrate depth, planting specification, installation method and the management and maintenance of the roof. The roof must then be installed in accordance with the approved strategy prior to first use of the extension and shall be maintained in accordance with the strategy in perpetuity unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to maintain the integrity and connectivity of the strategic green infrastructure network, a biodiversity net gain and ensure a satisfactory appearance to the finished building in accordance with Policies BCS9, BCS21, DM17, DM19, DM26, DM28 and DM29.

## 9. Further details: Construction/large scale elements

No development shall proceed above slab level until further details comprising construction sections and large scale detailed plans and elevations (to an appropriate scale) of the following elements, are submitted to and approved in writing by the Local Planning Authority.

- o Proposed chapel window reveals
- o Chapel clocks and clock tower roof
- o Chapel quoin details
- o Chapel dormers
- o Raised limestone window dressings
- o Extension typical windows
- o Extension typical doors
- o Extension stone/brick materials junction
- o Extension limestone banding
- o Extension roof eaves profile/barge board fascias
- o Residential entrances security measures and gates (main entrance and side access route) including technical specifications
- o Cycle store security measures and lighting including technical specifications

The development shall then be completed in full accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to ensure the finished appearance of the building is of a high quality and responds appropriately to the character and appearance of the local area, including conservation area, avoiding harm to heritage assets in accordance with Policies BCS21, BCS22, DM26, DM28, DM29, DM30 and DM31.

## 10. Further details: Stone work

No development shall proceed above slab level until further details of typical stone work including pennant and limestone areas are submitted to and approved in writing by the Local Planning Authority. Details of quarry/source, colour, texture, typical face bond and pointing shall be supplied including samples as necessary. The development shall then be completed

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in full accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to ensure the finished appearance of the building is of a high quality and responds appropriately to the character and appearance of the local area, including conservation area, avoiding harm to heritage assets in accordance with Policies BCS21, BCS22, DM26, DM28 and DM29, DM30 and DM31.

11. Further details: Materials

No development shall proceed above slab level until further details regarding proposed materials including manufacturer, specification, product information and samples (if necessary), demonstrating appearance, colour and texture of the following elements, are submitted to and approved in writing by the Local Planning Authority.

1. Red brick
2. Roof tiles
3. Coping stones
4. Cladding
5. Boundary walls
6. Paving
7. Privacy screens

The development shall then be completed in full accordance with the approved materials unless otherwise agreed in writing by the Local Planning Authority.

Reason: In order to ensure the finished appearance of the building is of a high quality and responds appropriately to the character and appearance of the local area, including conservation area, avoiding harm to heritage assets in accordance with Policies BCS21, BCS22, DM26, DM28 and DM29, DM30 and DM31.

12. Contract for Redevelopment

Works for the demolition of the building(s) or part of the building forming part of the development hereby permitted shall not be commenced before a valid contract for the carrying out and completion of works of redevelopment of the site for which planning permission has been granted has been entered into, and evidence of that contract submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure the demolition is followed by immediate rebuilding and to maintain the character and appearance of the Conservation Area.

13. Local Employment Opportunities

No development shall take place including any works of demolition until the developer/occupier enters into an agreement with the city council to produce and implement a strategy that aims to maximise the opportunities for local residents to access employment offered by the development. The approved strategy shall be undertaken in accordance with an agreed timetable.

Reason: In recognition of the employment opportunity offered by the early phases of the construction and operation of the development.

14. Protection of Retained Trees during the Construction Period

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No work of any kind shall take place on the site until the protective barriers have been erected around the retained trees, in the position and to the specification in section 5.4 of the Arboricultural Impact Assessment by Barton Hyett, dated March 2020. Once installed photos should be electronically sent to the Local Authority Case Officer, to be verified in writing by the Tree Officer. The Local Planning Authority shall be given not less than two weeks prior written notice by the developer of the commencement of works on the site in order that the council may verify in writing that the approved tree protection measures are in place when the work commences. The approved fence(s) shall be in place before any equipment, machinery or materials are brought on to the site for the purposes of the development and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Within the fenced area(s) there shall be no scaffolding, no stockpiling of any materials or soil, no machinery or other equipment parked or operated, no traffic over the root system, no changes to the soil level, no excavation of trenches, no site huts, no fires lit, no dumping of toxic chemicals and no retained trees shall be used for winching purposes. If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the Council.

Under no circumstances should the tree protection be moved during the period of the development and until all works are completed and all materials and machinery are removed.

Landscaping works within protected areas is to be agreed with the Local Planning Authority and carried out when all other construction and landscaping works are complete.

Reason: To protect the retained trees from damage during construction, including all ground works and works that may be required by other conditions, and in recognition of the contribution which the retained tree(s) give(s) and will continue to give to the amenity of the area in line with Policy DM17.

15. Sustainable Drainage System (SuDS)

No development shall take place until a Sustainable Drainage Strategy and associated detailed design, management and maintenance plan of surface water drainage for the site using SuDS methods has been submitted to and approved in writing by the Local Planning Authority. The approved drainage system shall be implemented in accordance with the approved Sustainable Drainage Strategy prior to the use of the building commencing and maintained thereafter for the lifetime of the development.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal is incorporated into the design and the build and that the principles of sustainable drainage are incorporated into this proposal and maintained for the lifetime of the proposal.

16. The Bat & Protected Species Survey (EcoLogic, 3rd April 2023 update Rev 03) is valid for 12 months only. If the works have not commenced within 18 months of the survey date, then the survey should be repeated and the results submitted to Bristol City Council for written approval in a report, prior to commencement. The survey report shall conclude whether the Ecological Mitigation and Enhancement Strategy should be updated, and if so, an updated EMES shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of any works.

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The development shall then be undertaken in full accordance with the approved Ecology report/EMES.

Reason: To ensure legal and policy compliance with regard to valued ecological species and habitats as well as to invasive plant species.

17. Sound Insulation between residential flats and retail units on ground floor

No development shall take place until a scheme of noise insulation measures for the partition between the residential accommodation and the retail units on the ground floor has been submitted to and approved in writing by the Council.

The scheme of noise insulation measures shall be prepared by a suitably qualified acoustic consultant/engineer and shall take into account the provisions of BS 8233: 2014 " Guidance on sound insulation and noise reduction for buildings.

The approved scheme shall be implemented prior to the commencement of the use and be permanently maintained thereafter.

Reason: To safeguard the amenity of future occupiers

**Pre occupation condition(s)**

18. Air source heat pump specifications

No building or use hereby permitted shall be occupied or the use commenced until a report detailing the air source heat pump specifications and predicted noise levels along with any necessary acoustic screening has been submitted to and been approved in writing by the Local Planning Authority.

Reason: In order to safeguard the amenities of adjoining residential occupiers.

19. Artificial Lighting (external)

No building or use hereby permitted shall be occupied or use commenced until a report detailing the lighting scheme and predicted light levels at neighbouring residential properties has been submitted to and been approved in writing by the Local Planning Authority.

Artificial lighting to the development must conform to requirements to meet the Obtrusive Light Limitations for Exterior Lighting Installations for Environmental Zone - E2 contained within Table 1 of the Institute of Light Engineers Guidance Notes for the Reduction of Obtrusive Lighting, GN01, dated 2005.

Reason: In order to safeguard the amenities of adjoining residential occupiers.

20. Further details: Bird/bat/bee boxes

Prior to first residential occupation of the extension hereby approved, detailed proposals must be submitted and approved in writing by the Local Planning Authority for the

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installation of one built-in house sparrow terrace, one built in swift brick, one built-in bee brick or box and two built-in bat boxes. Bird boxes shall be located on the north or east elevations of the extension. The location, specification, height and orientation of these features shall be shown on a site plan.

Bee bricks and bat boxes shall be located beneath eaves level on the south elevation of the building. Development shall be undertaken in accordance with the approved details unless otherwise agreed in writing by the Local Planning Authority.

Reason: To provide nesting opportunities for legally protected species and to ensure the development achieves a biodiversity net gain in accordance with national planning policy.

**21. Reporting of Unexpected Contamination**

In the event that contamination is found at any time when carrying out the approved development that was not previously identified, it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken and where remediation is necessary, a remediation scheme must be prepared and submitted for the approval of the Local Planning Authority.

Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

**22. Implementation of Approved Remediation Scheme**

No occupation of the development, shall take place until the approved remediation scheme has been carried out in accordance with its terms. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Following completion of measures identified in the approved remediation scheme, a verification report (otherwise known as a validation report) that demonstrates the effectiveness of the remediation carried out must be produced, and be approved in writing by the Local Planning Authority.

Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

**23. Implementation/Installation of Residential Refuse Storage and Recycling Facilities - Shown on Approved Plans**

No building or use hereby permitted shall be occupied or use commenced until the residential refuse store and area/facilities allocated for storing of recyclable materials, as shown on the approved plans have been completed in accordance with the approved plans.

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Thereafter, all refuse and recyclable materials associated with the development shall either be stored within this dedicated store/area, as shown on the approved plans, or internally within the building(s) that form part of the application site. No refuse or recycling material shall be stored or placed for collection on the adopted highway (including the footway), except on the day of collection.

Reason: To safeguard the amenity of the occupiers of adjoining premises; protect the general environment; prevent any obstruction to pedestrian movement and to ensure that there are adequate facilities for the storage and recycling of recoverable materials.

## 24. New works to match - Locally listed building

All new external and internal works and finishes, and any works of making good, shall match the existing original fabric in respect of using materials of a matching form, composition and consistency, detailed execution and finished appearance, except where indicated otherwise on the drawings hereby approved.

Reason: In order that the special architectural and historic interest of this locally listed building is safeguarded.

## 25. Completion and Maintenance of Cycle Provision - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the cycle parking provision shown on the approved plans has been completed, and thereafter, be kept free of obstruction and available for the parking of cycles only.

Reason: To ensure the provision and availability of adequate cycle parking.

## 26. Completion of Pedestrians/Cyclists Access - Shown on approved plans

No building or use hereby permitted shall be occupied or the use commenced until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

## 27. Installation of vehicle crossover - Shown on Approved Plans

No building or use hereby permitted shall be occupied or use commenced until drop kerbs has been installed at the carriageway edge and a vehicle cross-over constructed across the footway fronting the site in accordance with the approved plans and retained in that form thereafter for the lifetime of the development.

Reason: In the interests of pedestrian safety and accessibility

## 28. Completion and Maintenance of Car/Vehicle Parking - Shown on Approved Plans

No building or use hereby permitted shall be occupied or use commenced until the car/vehicle parking area shown on the approved plans has been completed and thereafter the area shall be kept free of obstruction and available for the parking of vehicles associated with the development.

The disabled parking bay shown on the approved plans shall be kept free of obstruction and available for the parking of residents registered disabled and allocated this space only.



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Reason: To ensure that there are adequate parking facilities to serve the development constructed to an acceptable standard.

## 29. Provision of Pedestrian Visibility Splays

No building or use hereby permitted shall be occupied or use commenced until pedestrian visibility splays of 2 metres x 2 metres to the rear of the footway, shall be provided at the vehicular access serving 4A Berkeley Road adjacent to the west of the site. Nothing shall be erected, retained, planted and/or allowed to grow at or above a height of 1 metre to the rear of the footway which would obstruct the visibility splay. The visibility splays shall be maintained free of obstruction at all times thereafter for the lifetime of the development.

Reason: To ensure motorists have clear and unrestricted views of approaching

## 30. Sound insulation of residential properties from external noise

All recommendations detailed in the Noise Assessments submitted with the application with regards to sound insulation and ventilation of residential properties shall be implemented in full prior to the commencement of the use permitted and be permanently maintained.

Reason: To ensure that the development achieves an acceptable standard of residential amenity for future occupiers.

## 31. Submission and Approval of Landscaping Scheme

No building or use hereby permitted shall be occupied or the use commenced until there has been submitted to and approved in writing by the Local Planning Authority a scheme of hard and soft landscaping, which shall include indications of all existing trees and hedgerows on the land, and details of any to be retained, together with measures for their protection, in the course of development. The approved scheme shall be implemented so that planting is carried out no later than the first planting season following the occupation of the building(s) or the completion of the development whichever is the sooner. All planted materials shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To protect and enhance the character of the site and the area, and to ensure its appearance is satisfactory.

**Post occupation management**

## 32. Transparent glazing

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order revoking and/or re-enacting that Order) the proposed glazing within the former Methodist Chapel at ground floor level adjacent to Gloucester Road shall be glazed with transparent (clear) glazing to a specification to be agreed with the Local Planning Authority and in accordance with all approved details and plans and shall be permanently maintained thereafter as transparent glazing.

Reason: To safeguard the visual amenity of the surrounding area and the activity of the designated Town Centre.

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## 33. Use Class Restriction

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) the ground floor commercial unit hereby approved shall only be used for retail purposes (use class A1/Class E(a)) and for no other use within of The Town and Country Planning (Use Classes) Order 1987 as amended 1st September 2020, or any provision equivalent to that Class in any Statutory Instrument revoking and/or re-enacting that Order).

Reason: This use only is permitted and other uses, either within the same Use Class, or permitted by the Town and Country Planning (General Permitted Development) Order 2015 as amended are not acceptable to the Local Planning Authority in this location due to the scale and location. Any other use of the site would require independent assessment in relation to the impacts associated with the alternative use.

## 34. Sustainability and energy efficiency measures

The development hereby approved shall incorporate the energy efficiency measures, renewable energy, sustainable design principles and climate change adaptation measures into the design and construction of the development in full accordance with the energy and sustainability statements (Sustainable Energy Statement Revision E- 20 April 2022) prior to first occupation. A total 20.5% reduction in carbon dioxide emissions below residual emissions through renewable technologies (solar panels) shall be achieved.

Reason: To ensure the development incorporates measures to minimise the effects of, and can adapt to a changing climate in accordance with Policies BCS13 (Climate Change), BCS14 (Sustainable Energy), BCS15 (Sustainable Design and Construction) and DM29 (Design of New Buildings).

## 35. Restriction of noise from plant and equipment

The rating level of any noise generated by plant & equipment as part of the development shall be at least 5 dB below the background level as determined by BS4142: 2014 Methods for rating and assessing industrial and commercial sound.

Reason: To safeguard the amenity of nearby premises and the area generally.

## 36. Hours of Deliveries (Class E use only)

Activities relating to deliveries shall only take place between 08.00 and 20.00 Monday to Saturday and not at all on Sundays or Bank Holidays.

Reason: to safeguard the amenities of neighbouring occupiers

## 37. Use of Refuse and recycling facilities

Activities relating to the collection of refuse and recyclables and the tipping of empty bottles into external receptacles shall only take place between 08.00 and 20.00 Monday to Saturday and not at all on Sundays or Bank Holidays.

Reason: To safeguard the amenities of nearby occupiers

## 38. Class E Use- Hours open to customers Monday - Saturday

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No customers shall remain on the retail premises (Use Class E) outside the hours of 08:00 to 23:00 on Monday to Saturday.

Reason: To safeguard the residential amenity of nearby occupiers.

## 39. Walls/Fences

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and/or re-enacting that Order) no fences, gates or walls shall be erected within the curtilage of the dwellinghouse(s) hereby permitted forward of any wall of the dwellinghouse(s) which fronts onto a road.

Reason: In the interests of visual amenity and the character of the area.

**List of approved plans**

## 40. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

3516 10B Proposed Site and Landscape Plan, received 9 March 2023  
 3516 11C Proposed South and West Elevations, received 9 March 2023  
 3516 1B Proposed Ground and First Floor Plans, received 9 March 2023  
 3516 2B Proposed Second Floor and Roof Plan, received 9 March 2023  
 3516 3C Proposed North and East Elevations, received 9 March 2023  
 3516 4C Proposed Berkeley Road Strip Elevation, received 9 March 2023  
 3516/ 3 Proposed bike store details, received 9 March 2023  
 PL01 Location plan, received 20 July 2021  
 PL15 Proposed section through link, received 20 July 2021  
 PL16 Proposed section through chapel, received 20 July 2021

Reason: For the avoidance of doubt.

**Advices**

## 1 Traffic Regulation Order (TRO)

You are advised that a Traffic Regulation Order (TRO) is required. You must submit a plan to a scale of 1:1000 of an indicative scheme for a TRO, along with timescales for commencement and completion of the development. Please be aware that the statutory TRO process is not straightforward; involving the public advertisement of the proposal(s) and the resolution of any objections.

You should expect a minimum of six months to elapse between the Highway Authority's TRO Team confirming that it has all the information necessary to enable it to proceed and the TRO being advertised. You will not be permitted to implement the TRO measures until the TRO has been sealed, and we cannot always guarantee the outcome of the process.

We cannot begin the TRO process until the appropriate fee has been received. To arrange for a TRO to be processed contact the Highway Authority's Transport Development Management Team at [transportdm@bristol.gov.uk](mailto:transportdm@bristol.gov.uk)

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N.B. The cost of implementing any lining, signing or resurfacing required by the TRO is separate to the TRO fees, which solely cover the administration required to prepare, consult, amend and seal the TRO.

2 Excavation Works on the Adopted Highway

The development hereby approved includes the carrying out of excavation works on the adopted highway. You are advised that before undertaking any work on the adopted highway you will require a Section 171 (Excavation) Licence from the Highway Authority which is available at [www.bristol.gov.uk/highwaylicences](http://www.bristol.gov.uk/highwaylicences)

3 Restriction of Parking Permits - Future Controlled Parking Zone/Residents Parking Scheme

You are advised that the Local Planning Authority has recommended to the Highways Authority that on the creation of any Controlled Parking Zone/Residents Parking Scheme area which includes the development, that the development shall be treated as car free / low-car and the occupiers are ineligible for resident parking permits as well as visitors parking permits if in a Residents Parking Scheme.

4 Stopping Up/Diversion of Adopted Highway

You are advised that to facilitate the development an order must be obtained to stop up or divert the adopted highway under sections 247 and 248 of the Town and Country Planning Act 1990. Please see [www.gov.uk/government/publications/stopping-up-and-diversion-of-highways](http://www.gov.uk/government/publications/stopping-up-and-diversion-of-highways) or contact the National Transport Casework Team at [nationalcasework@dft.gov.uk](mailto:nationalcasework@dft.gov.uk)

5 Works on the Public Highway

The development hereby approved includes the carrying out of work on the adopted highway. You are advised that before undertaking work on the adopted highway you must enter into a highway agreement under Section 278 of the Highways Act 1980 with the council, which would specify the works and the terms and conditions under which they are to be carried out.

Contact the Highway Authority's Transport Development Management Team at [transportDM@bristol.gov.uk](mailto:transportDM@bristol.gov.uk) allowing sufficient time for the preparation and signing of the Agreement. You will be required to pay fees to cover the council's costs in undertaking the following actions:

- I. Drafting the Agreement
- II. A Monitoring Fee equivalent to 15% of the planning application fee
- III. Approving the highway details
- IV. Inspecting the highway works

NB: Planning permission is not permission to work in the highway. A Highway Agreement under Section 278 of the Highways Act 1980 must be completed, the bond secured and the Highway Authority's technical approval and inspection fees paid before any drawings will be considered and approved.

6 Street Name and Numbering

You are advised that to ensure that all new properties and streets are registered with the

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emergency services, Land Registry, National Street Gazetteer and National Land and Property Gazetteer to enable them to be serviced and allow the occupants access to amenities including but not limited to; listing on the Electoral Register, delivery services, and a registered address on utility companies databases, details of the name and numbering of any new house(s) and/or flats/flat conversion(s) on existing and/or newly constructed streets must be submitted to the Highway Authority.

Any new street(s) and property naming/numbering must be agreed in accordance with the Councils Street Naming and Property Numbering Policy and all address allocations can only be issued under the Town Improvement Clauses Act 1847 (Section 64 & 65) and the Public Health Act 1925 (Section 17, 18 & 19). Please see [www.bristol.gov.uk/registeraddress](http://www.bristol.gov.uk/registeraddress)

7 Sustainable Drainage System (SUDS)

The development hereby approved includes the construction/provision of a sustainable drainage system. You are advised to contact the Highway Authority's Flood Risk Management Team at [flood.data@bristol.gov.uk](mailto:flood.data@bristol.gov.uk) before any works commence.

8 Application for advertisement consent needed: You are reminded of the need to obtain separate consent under the Town and Country Planning (Control of Advertisements) Regulations 1992 for any advertisements requiring express consent which you may wish to display on these premises.

9 Construction site noise: Due to the proximity of existing noise sensitive development and the potential for disturbance arising from contractors' operations, the developers' attention is drawn to Section 60 and 61 of the Control of Pollution Act 1974, to BS 5528: Parts 1 and 2: 2009 Noise and Vibration Control on Construction and Open Sites code of practice for basic information and procedures for noise and vibration control" and the code of practice adopted by Bristol City Council with regard to "Construction Noise Control". Information in this respect can be obtained from Pollution Control, City Hall, Bristol City Council, PO Box 3176, Bristol BS3 9FS.

10 Sound insulation/acoustic reports

The recommended design criteria for dwellings are as follows:

\* Daytime (07.00 - 23.00) 35 dB LAeq 16 hours in all rooms & 50 dB in outdoor living areas.

\* Nighttime (23.00 - 07.00) 30 dB LAeq 8 hours & LAmx less than 45 dB in bedrooms.

Where residential properties are likely to be affected by amplified music from neighbouring pubs or clubs, the recommended design criteria is as follows:

\* Noise Rating Curve NR20 at all times in any habitable rooms.

11 Noise - plant & equipment

Anti vibration mounts should be used to isolate plant from fixed structures and a flexible connector used to connect the flue to the fan if there is a potential to transmit vibration to any noise sensitive property. Any systems will also need regular maintenance so as to reduce mechanical noise.

12 Tree Protection: You are advised to refer to BS5837 : 2012 Trees in relation to construction for detailed information on types of tree protection, protection zones and other

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relevant matters.

- 13 Nesting birds: Anyone who takes, damages or destroys the nest of any wild bird whilst that nest is in use or being built is guilty of an offence under the Wildlife and Countryside Act 1981 and prior to commencing work you should ensure that no nesting birds will be affected.

- 14 Bats and bat roosts: Anyone who kills, injures or disturbs bats, obstructs access to bat roosts or damages or disturbs bat roosts, even when unoccupied by bats, is guilty of an offence under the Wildlife and Countryside Act 1981, the Countryside and Rights of Way Act 2000 and the Conservation (Natural Habitats, &c.) Regulations Act. Prior to commencing work you should ensure that no bats or bat roosts would be affected.

A grant of planning permission does not remove the legal protection afforded to bats and their roosts. If, during the works, any bats (or signs of bats, such as droppings) are found, an immediate halt should be called and a bat worker/ecologist should be consulted to determine if and how the works can proceed lawfully, with or without a mitigation licence.

- 15 Alterations to Vehicular Access

The development hereby approved includes the carrying out of alterations to vehicular access(s). You are advised that before undertaking work on the adopted highway you will require a Section 184 Licence from the Highway Authority which is available at [www.bristol.gov.uk/highwaylicences](http://www.bristol.gov.uk/highwaylicences)

The works shall be to the specification and constructed to the satisfaction of the Highways Authority. You will be required to pay fees to cover the Councils costs in undertaking the approval and inspection of the works.

- 16 Solar Photovoltaic System

The projected annual yield and technical details of the installed system will be provided by the Micro-generation Certification Scheme (MCS) approved installer.

The impact of shading on the annual yield of the installed PV system (the Shading Factor) should be calculated by an MCS approved installer using the Standard Estimation Method presented in the MCS guidance.

- 17 Wessex Water requirements: It will be necessary to comply with Wessex Water's main drainage requirements and advice and further information can be obtained from <http://www.wessexwater.co.uk>.

- 18 The proposed development lies within a coal mining area which may contain unrecorded coal mining related hazards. If any coal mining feature is encountered during development, this should be reported immediately to the Coal Authority on 0345 762 6848.

Further information is also available on the Coal Authority website at: [www.gov.uk/government/organisations/the-coal-authority](http://www.gov.uk/government/organisations/the-coal-authority)

- 19 Crime Prevention/ Security

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There is a large communal cycle store, these can be vulnerable to crime. We would recommend that the door providing access into the store should meet PAS 24:2016 and be incorporated into the access control system. The store must have cctv which provides 'identification' quality images in line with the Home Office document 28/09.

The building must have audio visual access control (with the facility to record images) and be compartmentalised to prevent the unlawful free movement through the building. Trades buttons must not be used.

Communal mail boxes must meet TS 009 standards.

Achieving the Secured by Design Award [www.securedbydesign.com](http://www.securedbydesign.com) would demonstrate that crime and disorder have been considered.

## Supporting Documents

102 Gloucester Road

1. Existing Site Plan
2. Existing Floor Plans
3. Existing North & East Elevations
4. Existing South & West Elevations
5. Proposed Site & Landscape Plan
6. Proposed Ground & First Floor Plan
7. Proposed Second Floor & Roof Plan
8. Proposed North & East Elevations
9. Proposed South & West Elevations
10. Proposed Section Through Chapel
11. Proposed Section Through link
12. Site Photos
13. Fuel Tanker Swept Path analysis
14. 20.00022.F – Approved Ground and First Floor Plans
15. 20.00022.F – Approved Second Floor and Roof Plans
16. 20.00022.F – Approved South and West Elevations
17. 20.00022.F – Approved North & East Elevations





PL02 Rev /  
Existing Site Plan

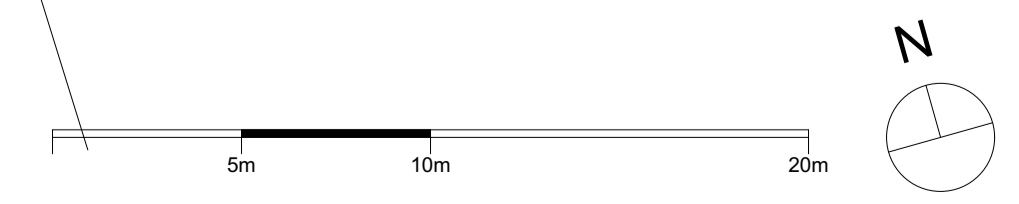
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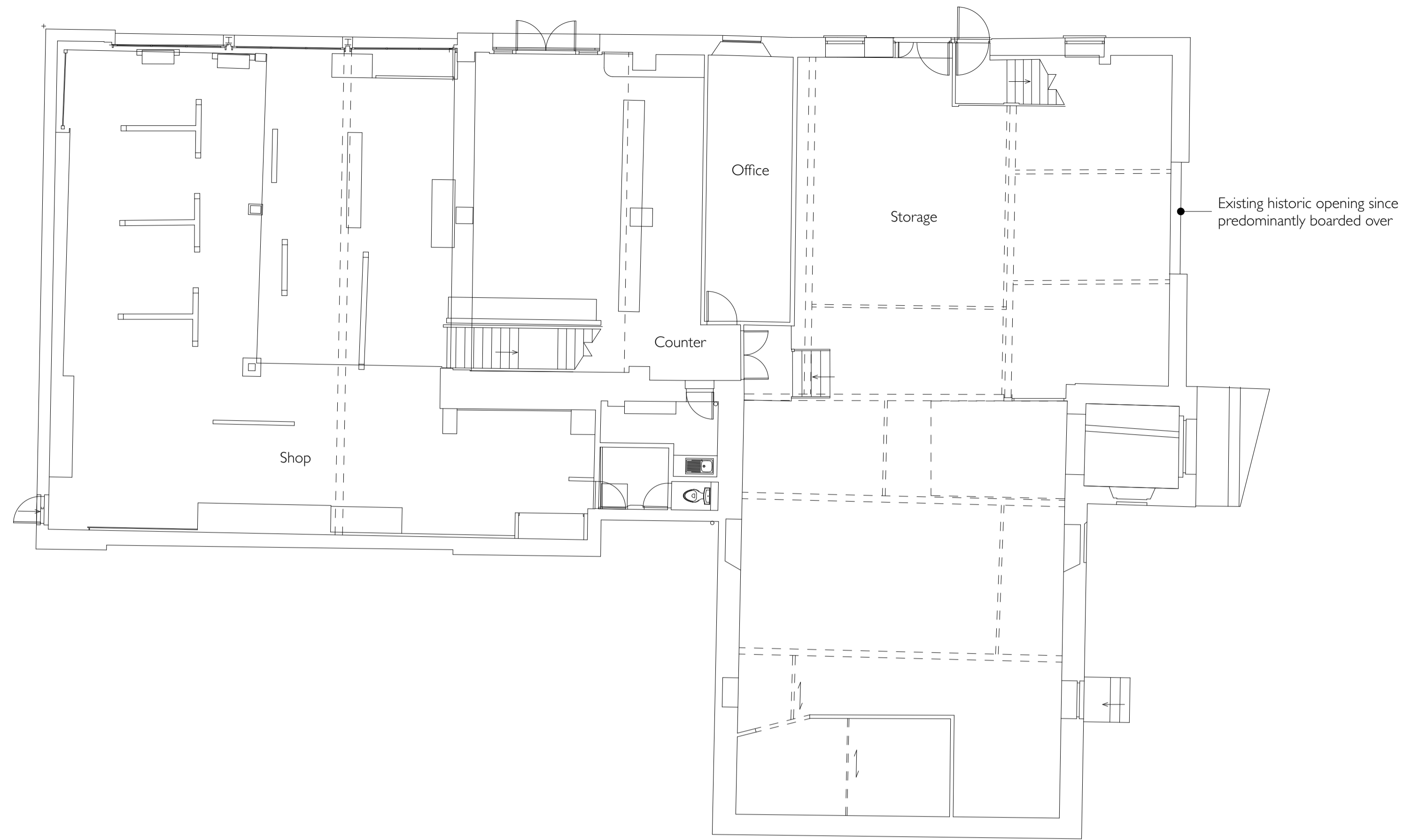
**NICHOLAS MORLEY ARCHITECTS LTD**

Box House, Bath Road, Box, Corsham, SN13 8AA

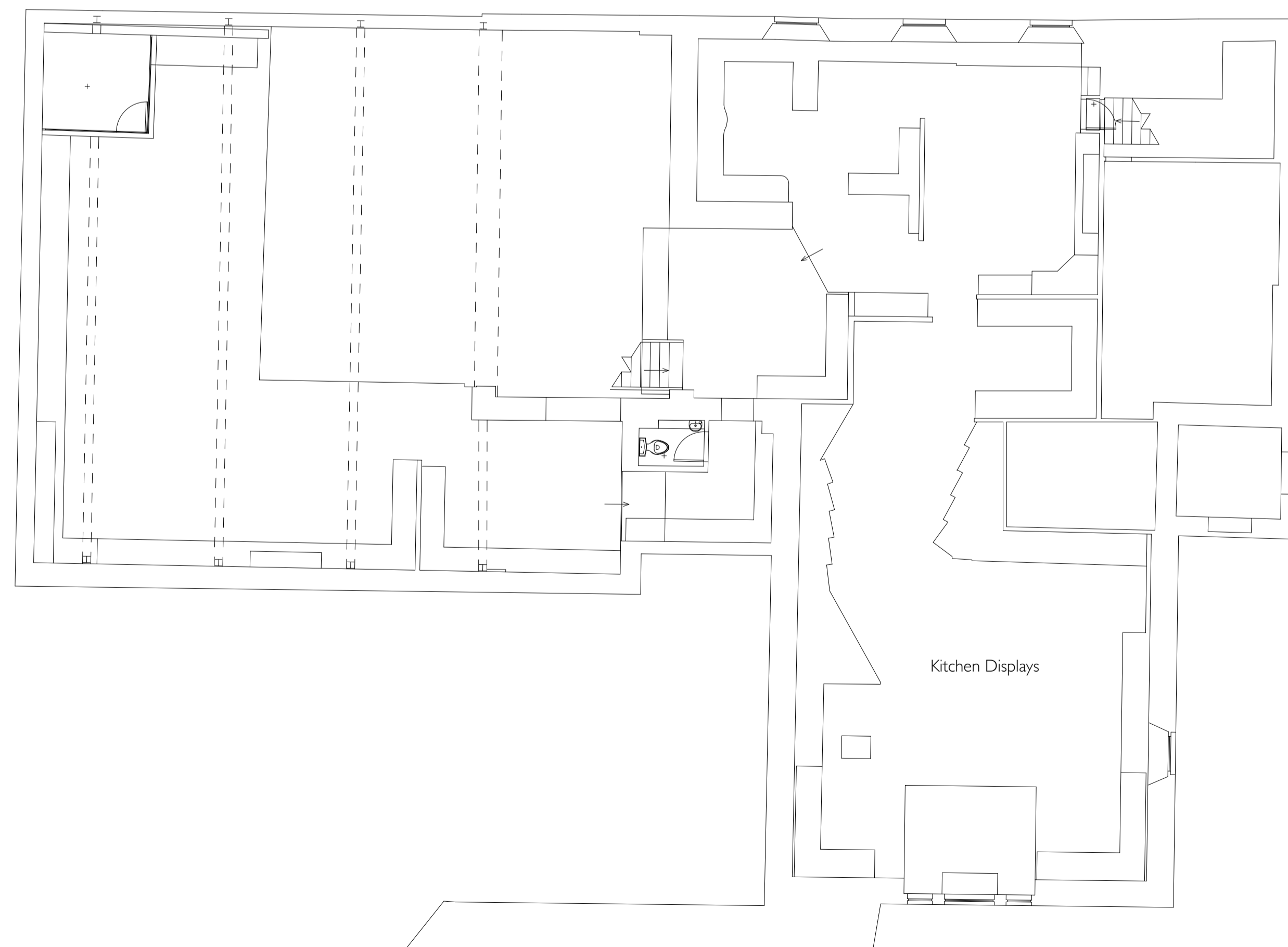
Email: [njm@njmarchitecture.co.uk](mailto:njm@njmarchitecture.co.uk)

[www.nicholasmorleyarchitects.co.uk](http://www.nicholasmorleyarchitects.co.uk)

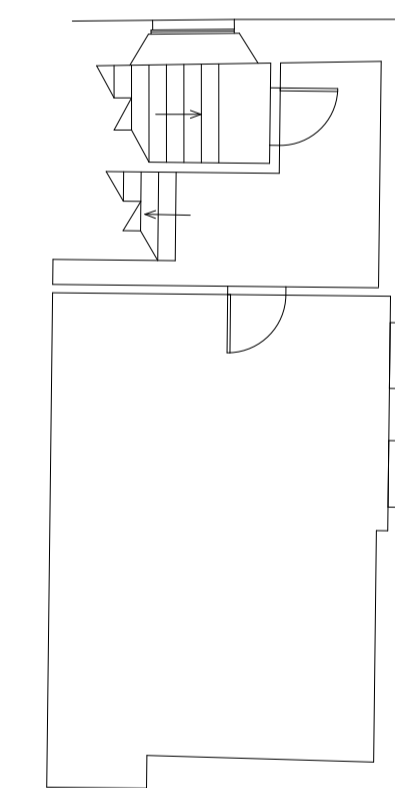




Existing **Ground Floor Plan**



Existing **First Floor Plan**



Existing **Upper First Floor Plan**

PL03 Rev /  
Existing Floor Plans

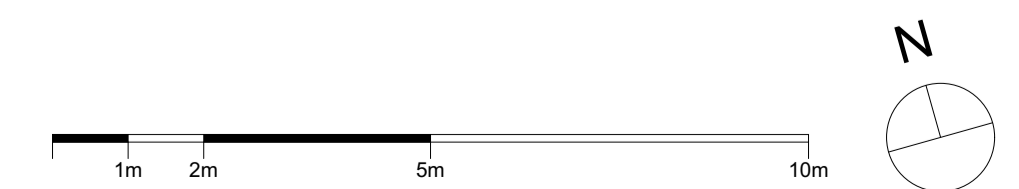
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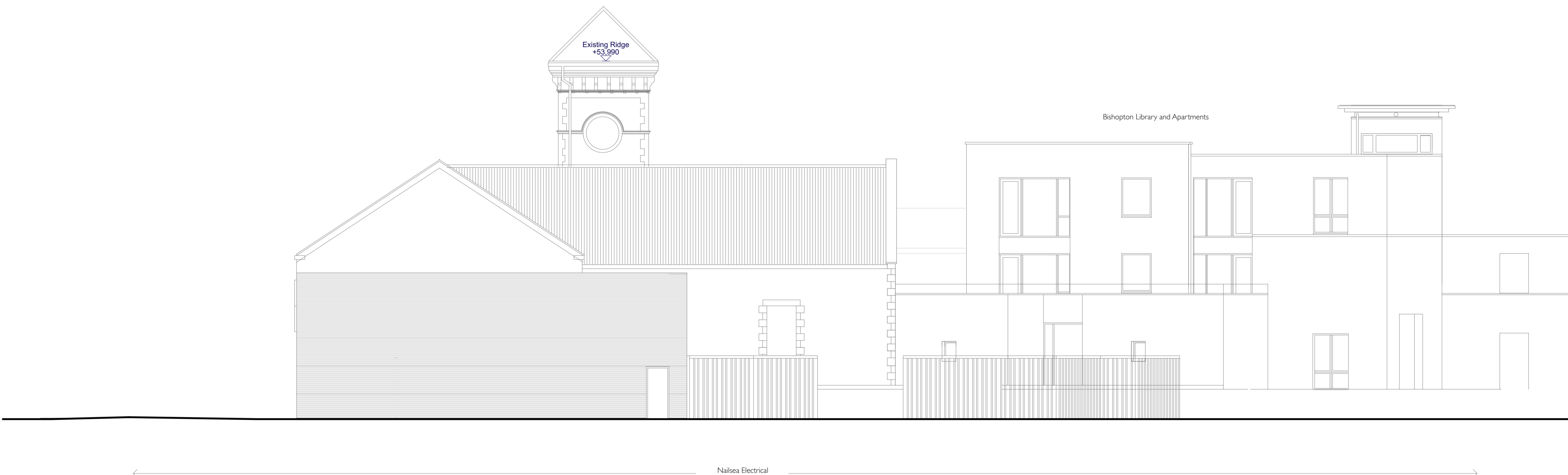
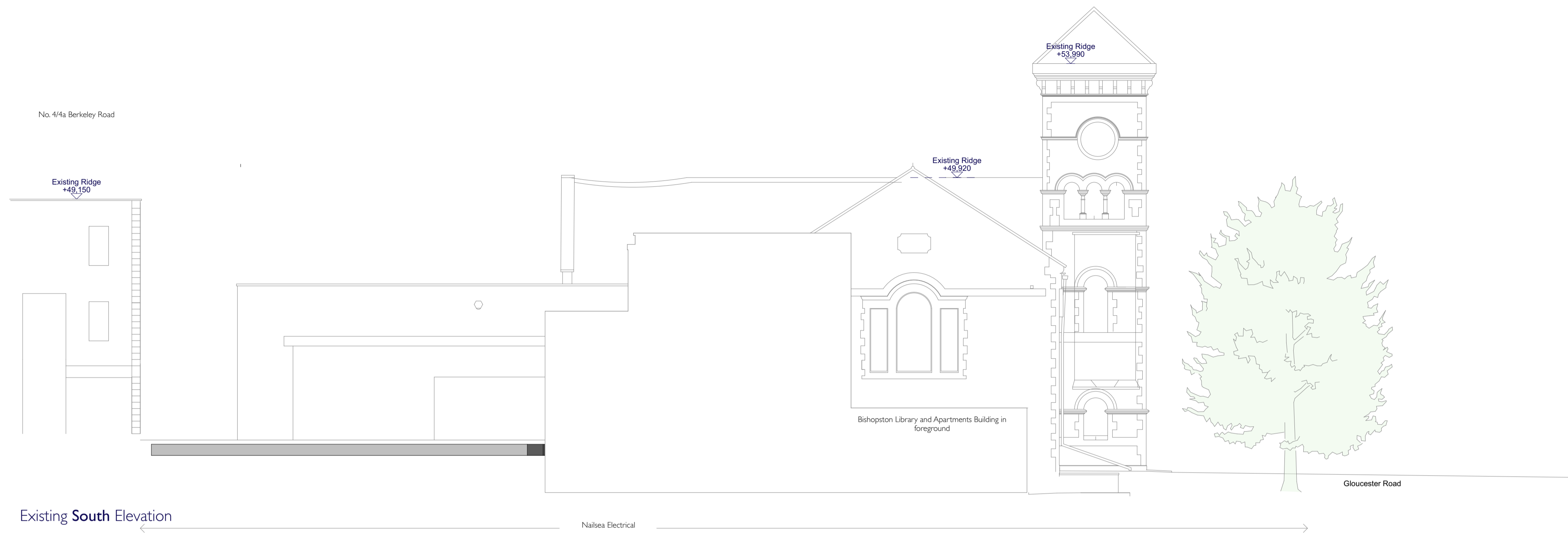


Revision History  
 Rev. A - 21/04/22 - Eastern elevation drawing corrected as drawing previously showed western elevation

PL04 Rev A  
 Existing North and East Elevations

1:100 @ A1

Existing East Elevation (Gloucester Road)



Revision History

Rev. A - 21/04/22 - Western elevation drawing corrected as drawing previously showed eastern elevation

Existing West Elevation

PL05 Rev A  
Existing South and West Elevations

1:100 @ A1



**PLANTING SPECIFICATION**

THE BELOW IMPLEMENTATION AND MAINTENANCE GUIDELINES ARE FOR PLANNING PURPOSES ONLY TO INDICATE THE LEVEL OF WORKMANSHIP TO BE SPECIFIED AND DOES NOT CONSTITUTE A DETAILED SPECIFICATION.

**1.0 GENERAL**

**1.1 RETAINED TREES**

ALL RETAINED TREES TO BE PROTECTED TO BS 5837. DO NOT DUMP SPOIL OR RUBBISH, EXCAVATE OR DISTURB TOPSOIL, PARK VEHICLES OR PLANT, STORE MATERIALS OR PLACE TEMPORARY ACCOMMODATION WITHIN THE BRANCH SPREAD. THE GROUND LEVEL WITHIN AN AREA OF 3 METRES BEYOND THE BRANCH SPREAD MUST NOT BE CHANGED WITHOUT PRIOR APPROVAL.

**1.2 PLANTING**

ALL PLANTS SHALL CONFORM TO BS 3936 AND BE IN ACCORDANCE WITH THE NATIONAL PLANT SPECIFICATION. SUPPLYING NURSERIES SHALL BE REGISTERED UNDER THE HTA NURSERY CERTIFICATION SCHEME. ALL PLANTS SHALL BE PACKED AND TRANSPORTED IN ACCORDANCE WITH THE CODE OF PRACTICE FOR PLANT HANDLING AS PRODUCED BY CPSE. PLANTING SHALL NOT BE CARRIED OUT WHEN THE GROUND IS WATERLOGGED, FROST BOUND OR DURING PERIODS OF COLD DRYING WINDS.

**1.3 TOPSOIL**

TOPSOIL TO BS 3882, OF EITHER GENERAL PURPOSE OR PREMIUM GRADE. TOPSOIL DEPTHS ARE TO BE 300MM FOR TREES AND SHRUBS AND 150MM FOR GRASS. IF THE FORMATION LEVEL IS COMPACTED IT SHOULD BE RIPPED THROUGH BEFORE TOPSOILING. TOPSOIL IS TO BE SPREAD OVER SUBSOIL IN LAYERS NOT EXCEEDING 150MM. GENTLY FIRM EACH LAYER BEFORE SPREADING THE NEXT. DO NOT COMPACT TOPSOIL. PRESERVE FRIABLE TEXTURE OF SEPARATE CRUMBS WHERE EVER POSSIBLE.

**1.4 OPERATIVES**

ALL LANDSCAPE OPERATIVES WILL BE APPROPRIATELY TRAINED, CERTIFIED AND QUALIFIED TO UNDERTAKE THE TASKS REQUIRED. WHEN REQUIRED, THE RELEVANT CERTIFICATES WILL BE MADE AVAILABLE FOR INSPECTION. ALL WORK IS TO BE CARRIED OUT IN ACCORDANCE WITH THE RELEVANT BRITISH STANDARDS, CODES OF PRACTICE AND LEGISLATION.

**2.0 TREE AND SHRUB PLANTING**

**2.1 GROUND PREPARATION**

WHERE NECESSARY TREAT EXISTING WEED GROWTH, BRAMBLES AND REGENERATIVE MATERIAL WITH A GLYPHOSATE BASED HERBICIDE AND ALLOW A SUITABLE PERIOD AS RECOMMENDED BY THE MANUFACTURER FOR THIS TO TAKE EFFECT. GENERAL PURPOSE SLOW RELEASE FERTILISER AT THE RATE OF 75GM/M<sup>2</sup> AND TREE PLANTING AND MULCHING COMPOST AT THE RATE OF 20LITRES/M<sup>2</sup> ARE TO BE INCORPORATED INTO THE TOP 150MM OF TOPSOIL DURING FINAL CULTIVATIONS. BREAK UP COMPACTED TOPSOIL TO FULL DEPTH AND ALL EXTRANEOUS MATTER SUCH AS PLASTIC, WOOD, METAL AND STONES GREATER THAN 50MM IN ANY DIMENSION SHALL BE REMOVED FROM SITE.

PLANT ONLY DURING THE FOLLOWING SEASONS: CONTAINER GROWN PLANTS: AT ANY TIME IF GROUND AND WEATHER CONDITIONS ARE FAVOURABLE. DECIDUOUS TREES: NOV TO LATE MAR. EVERGREENS: SEPT/OCT OR APR/MAY. HERBACEOUS: SEPT/OCT OR MAR/APR.

TREE PITS OF AT LEAST 75MM DIAMETER GREATER THAN THE ROOT SYSTEM AND NO DEEPER THAN THE ROOTBALL / CONTAINER DEPTH ARE TO BE EXCAVATED WITH THE SIDES WELL SCARIFIED TO PREVENT SMEARING. ALL CONTAINER GROWN AND TREES OVER HEAVY STANDARD SIZE SHALL BE DOUBLE STAKED. STAKES (SHORT DOUBLE FOR EXTRA HEAVY STANDARD) SHOULD BE DRIVEN 300MM INTO UNDISTURBED GROUND BEFORE PLANTING THE TREE, TAKING CARE TO AVOID UNDERGROUND SERVICES AND CABLES ETC. AND TIED WITH RUBBER TIES WITH SOLID RUBBER SPACER OF ADEQUATE WIDTH FOR THE TYPE OF TREE TO BE SECURED.

**2.2 PLANTING**

ALL CONTAINER GROWN ROOT SYSTEMS AND ROOTBALLS SHALL BE THOROUGHLY SOAKED AND ALL CONTAINERS AND ROOTBALL WRAPPINGS REMOVED PRIOR TO PLANTING.

THE TREE SHOULD BE PLANTED AT THE CORRECT DEPTH TAKING INTO ACCOUNT THE POSITION OF THE ROOT FLARE AND THE FINISHED LEVEL. THE ROOTBALL OR ROOT STEM TRANSITION SHOULD BE LEVEL WITH THE EXISTING HOST SOIL OR SURFACE. THE BASE OF THE ROOTBALL SHOULD TYPICALLY SIT ON SUBSOIL OR LARGER ROOTBALLS. THE SUBSOIL WILL SIT AROUND THE LOWER PORTION OF THE ROOTBALL.

TREE PITS SHOULD BE BACKFILLED WITH THE EXCAVATED TOPSOIL. THE ORIGINAL TOP SOIL IS NOT AVAILABLE OR DEEMED UNSUITABLE. A MULTI-PURPOSE TOPSOIL SHOULD BE USED. ANY SUBSOIL EXCAVATED SHOULD BE DISCARDED AND THE SUBSOIL DEPTH (BEYOND 300MM DEEP) BACKFILLED WITH A HIGH SAND CONTENT SUBSOIL. BACKFILL SHOULD BE ADDED GRADUALLY IN LAYERS OF 150MM TO 230MM DEPTH, ENSURING THE TREE IS HELD UPRIGHT AT EACH STAGE. THE FILL SHOULD BE FIRMED IN TO ELIMINATE ALL AIR POCKETS UNDER AND AROUND THE ROOT SYSTEM, BUT WITH CARE BEING TAKEN NOT TO EXCESSIVELY COMPACT THE SOIL. THE FINAL LAYER SHOULD NOT BE CONSOLIDATED.

GENERAL PURPOSE SLOW RELEASE FERTILISER (AT THE RATE OF 75GM/M<sup>2</sup>) AND TREE PLANTING AND MULCHING COMPOST AT THE RATE OF (20LITRES/M<sup>2</sup>) ARE TO BE INCORPORATED INTO THE TOP 150MM OF TOPSOIL DURING FINAL CULTIVATIONS. APPLY MEDIUM GRADE BARK MULCH (25-50MM), FREE OF PESTS, DISEASE FUNGUS AND WEEDS, TO AN AREA OF 1 METRE DIAMETER AROUND THE BASE OF EACH TREE.

SHRUBS AND HERBACEOUS PLANTS ARE TO BE SET OUT AS SHOWN ON THE DRAWING, BEING SPACED OUT EVENLY, SO THAT WHEN ESTABLISHED THEY COMPLETELY FILL THE AREA INDICATED AS PRECISELY AS POSSIBLE. PITS PLANTED INTO THE PREPARED SOIL AT THE SPECIFIED CENTRES WITH MINIMAL DISTURBANCE TO THE ROOTBALL AND WELL FIRMED IN. APPLY TOP DRESSING SLOW RELEASE FERTILISER EVENLY AT A RATE 100GM/M<sup>2</sup> OVER THE PLANTED BED.

SPREAD MEDIUM GRADE BARK MULCH (25-50MM), FREE OF PESTS, DISEASE FUNGUS AND WEEDS, TO A DEPTH OF 75MM ACROSS ALL NEW PLANTING AREAS, ENSURING GROUND COVER PLANTS ARE NOT BURIED.

WATER IN ALL TREES AND SHRUBS / HERBACEOUS PLANTS, AS NECESSARY TO ENSURE THE ESTABLISHMENT AND CONTINUED THRIVING OF ALL PLANTING, AT THE END OF EACH DAY OF PLANTING.

**2.3 MAINTENANCE**

THE LANDSCAPE CONTRACTOR SHALL MAINTAIN ALL AREAS OF NEW PLANTING FOR A PERIOD OF 12 MONTHS FOLLOWING PRACTICAL COMPLETION. ALL STOCK DEEMED TO BE DEAD, DYING OR DISEASED WITHIN THE DEFECTS PERIOD SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN COST.

THE SITE IS TO BE VISITED MONTHLY THROUGHOUT THE YEAR TO UNDERTAKE THE FOLLOWING OPERATIONS.

WEED CLEARANCE: ALL PLANTING AREAS ARE TO BE KEPT WEED FREE BY HAND WEEDING OR HERBICIDE TREATMENT.

LITTER CLEARANCE: ALL LITTER IS TO BE REMOVED FROM PLANTING BEDS.

WATERING: ALL SHRUBS ARE TO BE WATERED DURING THE GROWING SEASON FOLLOWING ANY DRY PERIODS OF 7 DAYS. PLANTING AREAS ARE TO BE BROUGHT UP TO FIELD CAPACITY AT EACH VISIT, ENSURING FULL DEPTH OF TOPSOIL IS SATURATED, AND EACH TREE IS TO RECEIVE 80 LITRES. CHECKING TREES: ALL TREE TIES AND STAKES ARE TO BE CHECKED AND ADJUSTED IF TOO LOOSE/TIGHT OR IF CHAFFING IS OCCURRING. ANY BROKEN STAKES ARE TO BE REPLACED. STAKES TO BE REMOVED WHEN TREES ARE ESTABLISHED.

FORMATIVE PRUNING: ANY DAMAGED SHOOTS/BRANCHES ARE TO BE PRUNED BACK TO HEALTHY WOOD. PLANTS ARE TO BE PRUNED IN ACCORDANCE WITH GOOD HORTICULTURAL PRACTICE TO MAINTAIN HEALTHY WELL-SHAPED SPECIMENS.

**3.0 STRUCTURE MIX**

**3.1 GROUND PREPARATION**

CLEAR SURFACE VEGETATION AND REMOVE ARISING. WHERE PLANTING IN EXISTING GRASS, CUT EXISTING ROUGH GRASS AND WEEDS BETWEEN 20MM AND 30MM AND REMOVE 300X300MM SQUARES OF TURF AT 1 METRE CENTRES FOR STRUCTURE MIX.

**3.2 PLANTING**

ALL BARE ROOTED STOCK IS TO BE ROOT DIPPED IN WATER RETAINING POLYMER. TRANSPLANTS ARE TO BE PIT PLANTED AND PROTECTED FROM RABBIT DAMAGE BY USING USING APPROVED PROPPRIETARY TREE AND SHRUB SHELTERS 900MM AND 900MM HIGH RESPECTIVELY. SECURED WITH STAKES AND TIES AS ADVISED BY THE MANUFACTURER. ALL EXTRANEOUS MATTER SUCH AS PLASTIC, WOOD, METAL AND STONES GREATER THAN 50MM IN ANY DIMENSION SHALL BE REMOVED FROM SITE.

**3.3 MAINTENANCE**




USING APPROVED HERBICIDES, A 1 METRE DIAMETER CIRCLE CENTRED ON EACH PLANTING STATION SHALL BE KEPT WEED FREE THROUGHOUT THE MAINTENANCE PERIOD. ANY WEEDS GROWING IMMEDIATELY ADJACENT TO PLANT STEMS SHALL BE REMOVED BY HAND. THE LANDSCAPE CONTRACTOR SHALL MAINTAIN ALL AREAS OF NEW WOODLAND AND STRUCTURE PLANTING FOR A PERIOD OF 12 MONTHS FOLLOWING PRACTICAL COMPLETION. IN THE AUTUMN FOLL PLANTING THE CA WILL PREPARE A LIST OF ALL PLANTS WHICH ARE DEAD, DYING OR DISEASED AND ARE TO BE REPLACED DURING THE FOLLOWING PLANTING SEASON. ALL STOCK DEEMED TO BE DEAD, DYING OR DISEASED WITHIN THE DEFECTS PERIOD SHALL BE REPLACED BY THE CONTRACTOR AT THEIR OWN COST.

**Proposed Hard Landscaping Notes:**

New permeable paving to site frontages on Berkeley Road and Gloucester Road. Light grey paving to parking areas, dark red/brown paving to residential entrance areas. To be laid in herringbone pattern.

Residential entrances delineated with 600mm high pennant stone walls (matching existing chapel)

New native planting as indicated on site plan and street scene drawings.

-  Indicates existing footpath on Berkeley Road to be resurfaced in tarmac with existing drop kerbs and remaining kerbs reset.
-  Indicates area to be re-paved in paving matching the existing paving outside Bishopston Library and existing kerbs to be reset
-  Pink/blue lines indicate extent of new pennant kerb to be constructed along Berkeley Road. Blue line indicates dropped kerb

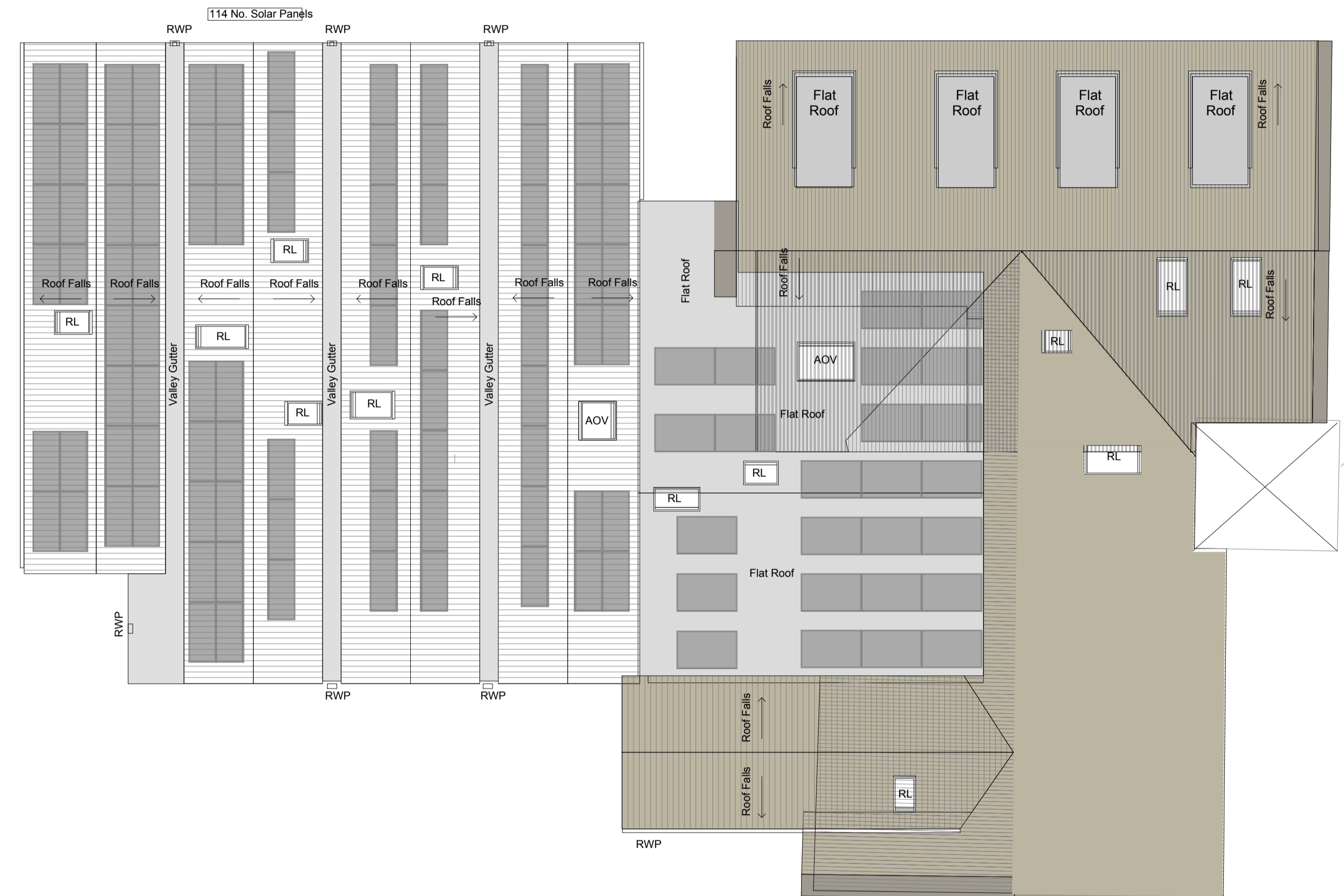
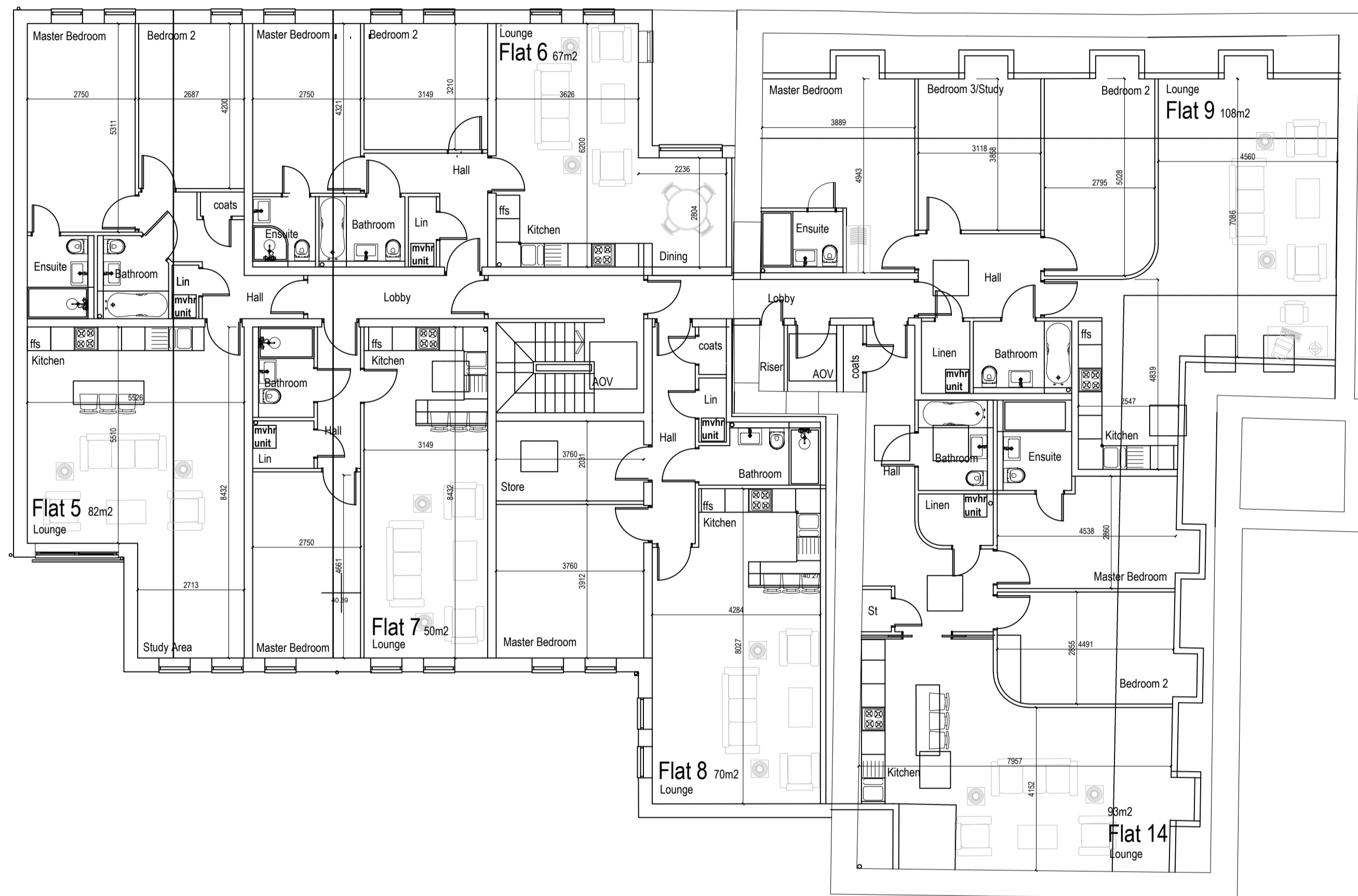


|      |          |                 |  |         |   |
|------|----------|-----------------|--|---------|---|
| Rev: | Date:    | Note:           | <b>Proposed Residential Conversion and Development</b><br><b>102 Gloucester Road</b><br><b>Bishopston Bristol</b><br><b>Proposed Site Plan</b> | Date:   | July 2022   |
| A    | Oct 2022 | General updates |  | Scale:  | 1:200 @ A1  |
| B    | Oct 2022 | General updates | DWG No:  | 3516/10 | David Cahill<br>Design Consultants Ltd<br>Unit 2 Office 4 Tower Lane Business Park<br>Warmley Bristol BS30 8XT<br>Tel: 01179618888<br>Email: davidcahill@design@btopenworld.com |





|        |                |                       |  |                   |  |
|--------|----------------|-----------------------|--|-------------------|--|
| Rev. A | Date: Oct 2022 | Note: General updates | <b>Proposed Residential Conversion and Development</b><br><b>102 Gloucester Road</b><br><b>Bishopston Bristol</b><br><b>Proposed Ground and First Floor Plan</b> | Date: July 2022   | <b>David Cahill</b><br>Design Consultants Ltd<br>Unit 2 Office 4 Tower Lane Business Park<br>Warmley Bristol BS30 8XT<br>Tel: 01179618888<br>Email: davidcahilldesign@topenworld.com |
| B      | Date: Oct 2022 | Note: General updates |  | Scale: 1:100 @ A1 |  |



|      |          |                 |   |   |
|------|----------|-----------------|---|---|
| Rev: | Date:    | Note:           | Proposed Residential Conversion and Development | Date:                                     |
| A    | Oct 2022 | General updates | <b>102 Gloucester Road</b>                      | July 2022                                 |
| B    | Oct 2022 | General updates | <b>Bishopston Bristol</b>                       | Scale:                                    |
|      |          |                 | <b>Proposed Second and Roof Plan</b>            | 1:100 @ A1                                |
|      |          |                 |   | DWG No:                                   |
|      |          |                 |   | <b>3516/2</b>                             |
|      |          |                 |   | David Cahill                              |
|      |          |                 |   | Design Consultants Ltd                    |
|      |          |                 |   | Unit 2 Office 4 Tower Lane Business Park  |
|      |          |                 |   | Warmley Bristol BS30 8XT                  |
|      |          |                 |   | Tel: 01179618888                          |
|      |          |                 |   | Email: davidcahill@design@btopenworld.com |



- Key for New Building Works:**
- 1 Red brickwork
  - 2 Porch roof to residential entrances with flat roof with dark grey metal perimeter flashings
  - 3 Dark brown double roman roof tiles
  - 4 Bath stone banding to external walls
  - 5 Dark Grey PPC Aluminium glazing system, indicated
  - 6 Dark grey metal rainwater goods
  - 7 Dark brown double roman tiles matching existing chapel
  - 8 Dark grey metal signage
  - 9 New pennant stone matching existing chapel with stone surrounds to new openings.
  - 10 Dark grey metal acoustic Louvre vents
- Key for Proposed Works to Existing Chapel:**
- A New dormer window to existing Chapel
  - B New dark red/brown double roman roof tiles
  - C Existing boarded up window/door to be re-opened and restored with new/repainted window frame and new glazing. Stonework restored where needed
  - D Existing opening to be restored and filled increased
  - E New rooflight in chapel roof (behind tower)
  - F Existing non-original doors removed and new double doors with dark grey metal frame proposed
  - G constructed. All materials to match existing chapel materials
- Note - All damaged/dilapidated stonework to chapel is to be repaired and restored



| Rev. | Date     | Note                          | Proposed Residential Conversion and Development |  | Date       |
|------|----------|-------------------------------|---|--|------------|
| A    | Oct 2022 | General updates               | 102 Gloucester Road                             |  | July 2022  |
| B    | Oct 2022 | acoustic Louvre vents added   | Bishopston Bristol                              |  | Scale:     |
| C    | Jan 2023 | acoustic Louvre vents removed | Proposed Elevations                             |  | 1:100 @ A1 |
|      |          |                               |   |  | DWG No:    |
|      |          |                               |   |  | 3516/3     |

|  |  |
|--|--|
| <b>David Cahill</b><br>Design Consultants Ltd<br>Unit 2 Office 4 Tower Lane Business Park<br>Warmley Bristol BS30 8XT<br>Tel: 01179618888<br>Email: davidcahilldesign@topenworld.com |  |
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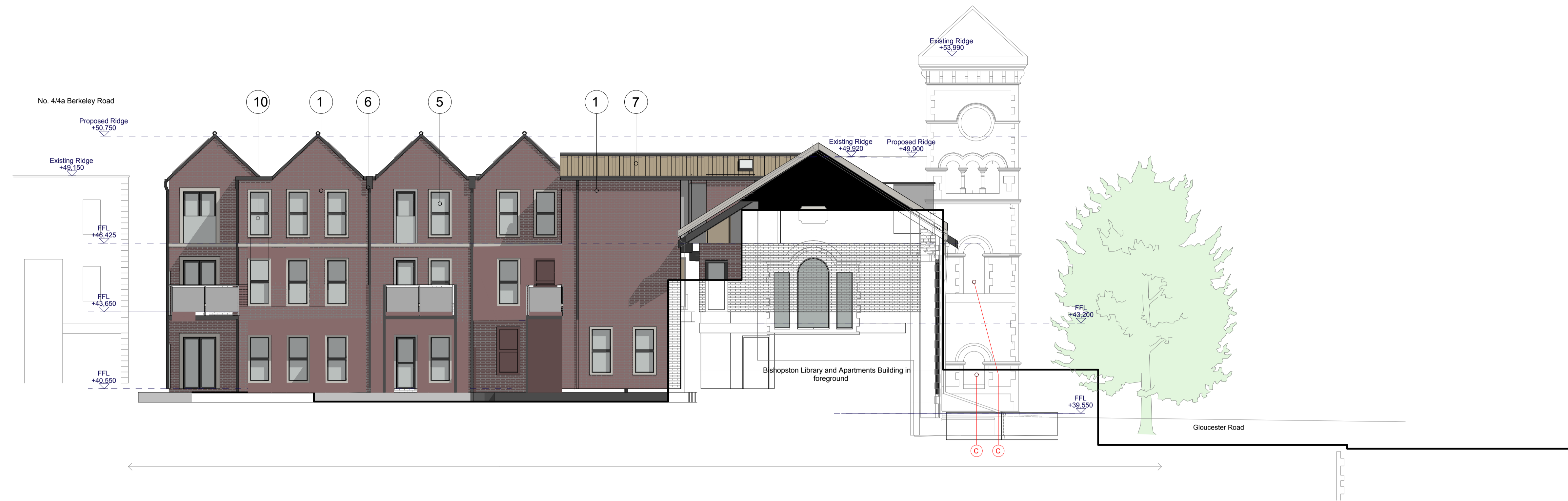
**Key for New Building Works:**

- 1 Red brickwork
- 2 Porch roof to residential entrances with flat roof with dark grey metal perimeter flashings and dark grey metal column support
- 3 Dark brown double roman roof tiles
- 4 Bath stone banding to external walls
- 5 Dark Grey PPC Aluminium glazing system. Windows to have stone surround where indicated
- 6 6 Dark grey metal rainwater goods
- 7 Dark brown double roman tiles matching existing chapel
- 8 8 Dark grey metal signage
- 9 New pennant stone matching existing chapel with stone surrounds to new openings.
- 10 Dark grey metal acoustic Louvre vents

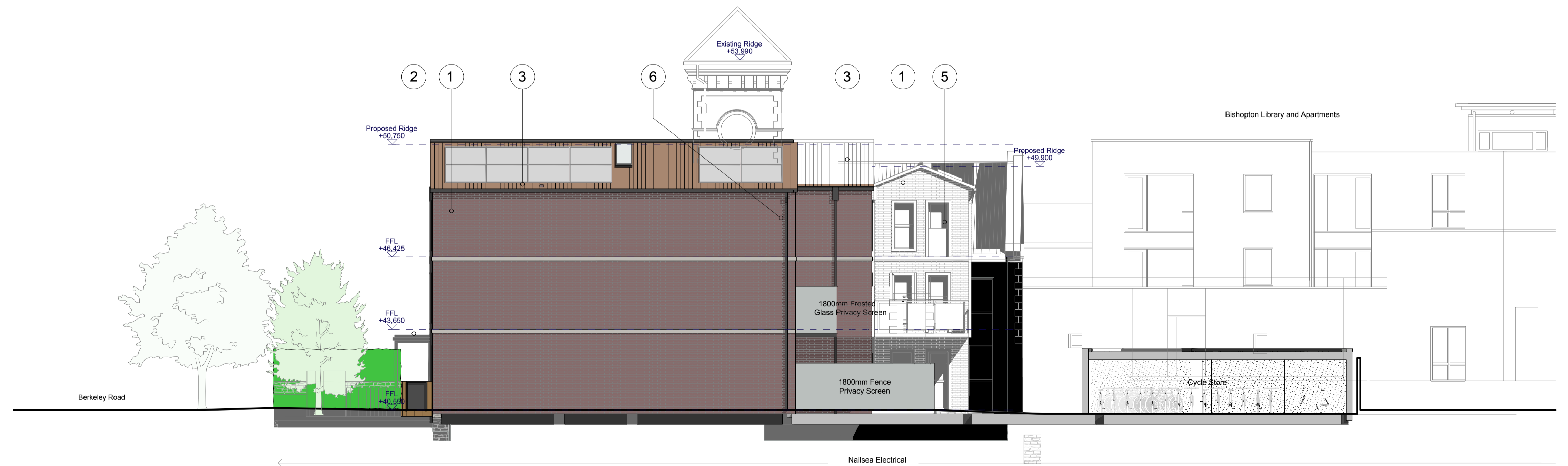
**Key for Proposed Works to Existing Chapel:**

- A New dormer window to existing Chapel
- B New dark red/brown double roman roof tiles
- C Existing boarded up window/door to be re-opened and restored with new/repainted window frame and new glazing. Stonework window surround to be repaired and restored where needed
- D Existing opening to be restored and filled with new glazing. Size of existing opening increased
- E New rooflight in chapel roof (behind tower)
- F Existing non-original doors removed and new double doors with dark grey metal frame proposed
- G New gable, coping and parapet barrier constructed. All materials to match existing chapel materials

Note - All damaged/dilapidated stonework to chapel is to be repaired and restored



Proposed South Elevation



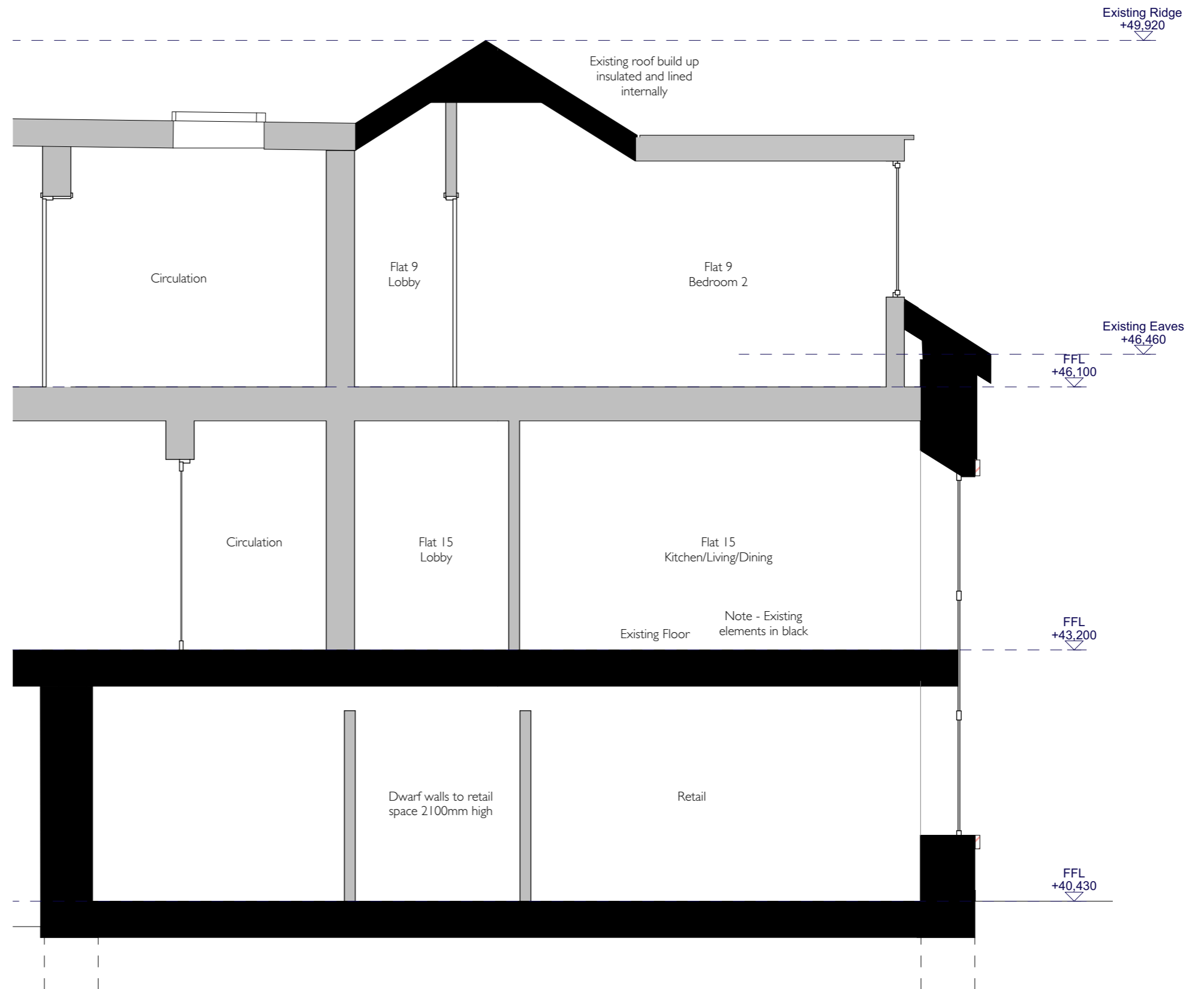
Proposed West Elevation

| Rev. | Date     | Note                          |
|------|----------|-------------------------------|
| A    | Oct 2022 | General updates               |
| B    | Oct 2022 | General updates               |
| C    | Jan 2023 | acoustic Louvre vents removed |

Proposed Residential Conversion and Development  
**102 Gloucester Road**  
**Bishopston Bristol**  
**Proposed Elevations**

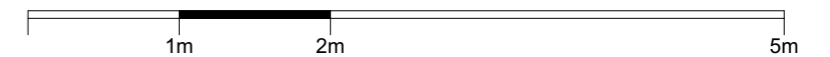
Date: July 2022  
 Scale: 1:100 @ A1  
 DWG No: 3516/11

**David Cahill**  
 Design Consultants Ltd  
 Unit 2 Office 4 Tower Lane Business Park  
 Warmley Bristol BS30 8XT  
 Tel: 01179618888  
 Email: davidcahilldesign@topenworld.com



PL16 Rev /  
Proposed Section Through Chapel

Scale 1:50 @ A3



PL15 Rev /  
Proposed Section Through Link and Elevation of Chapel Gable

Scale 1:50 @ A3

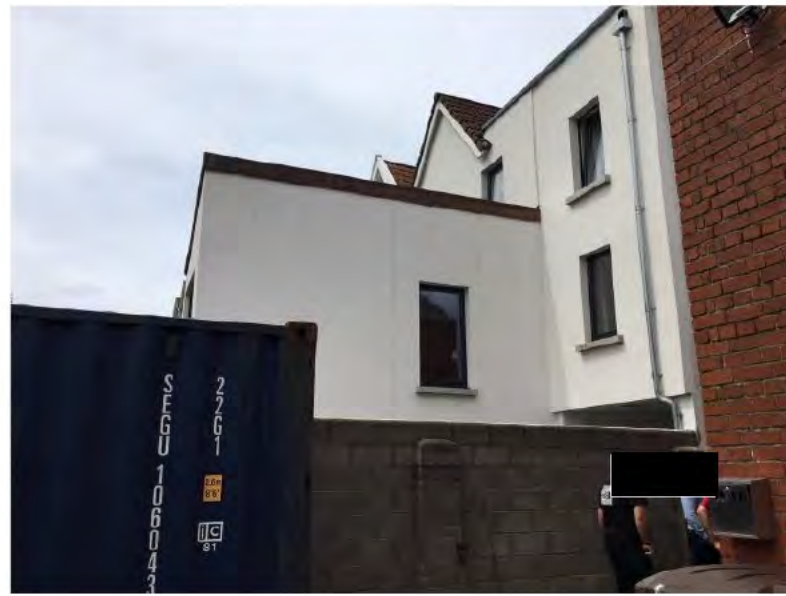




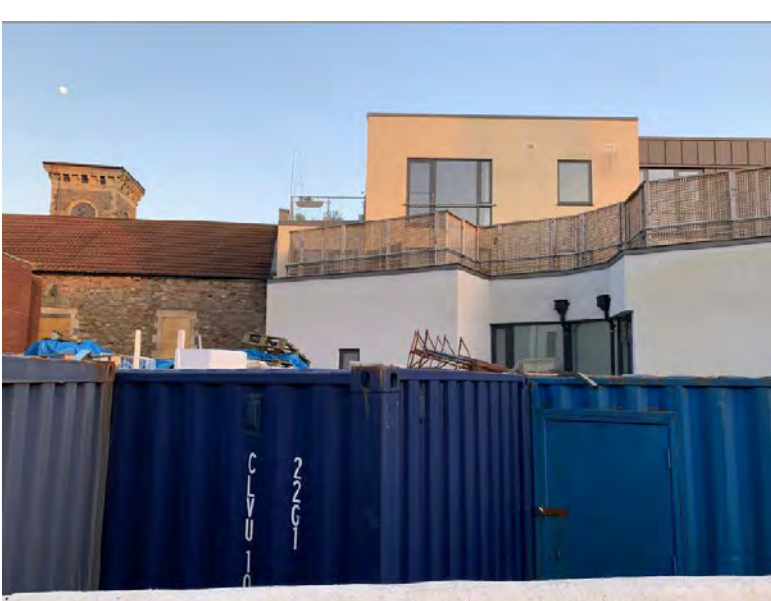
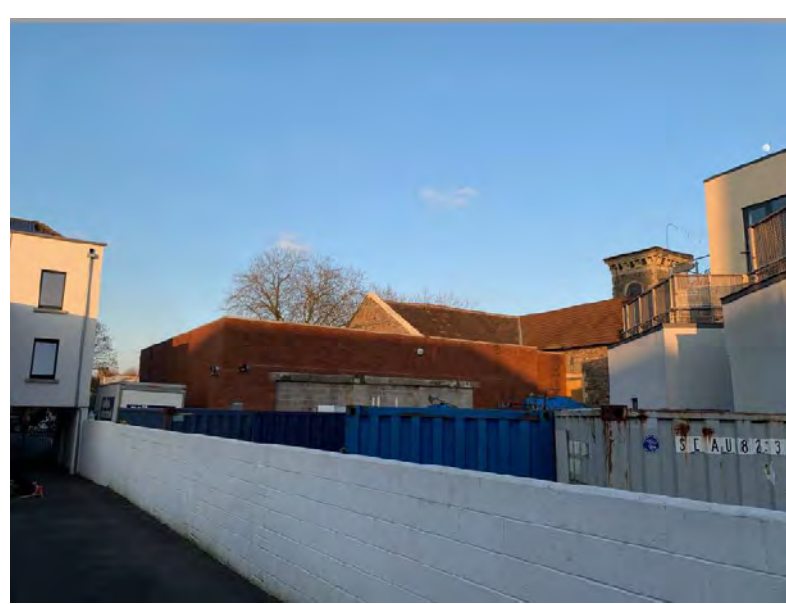
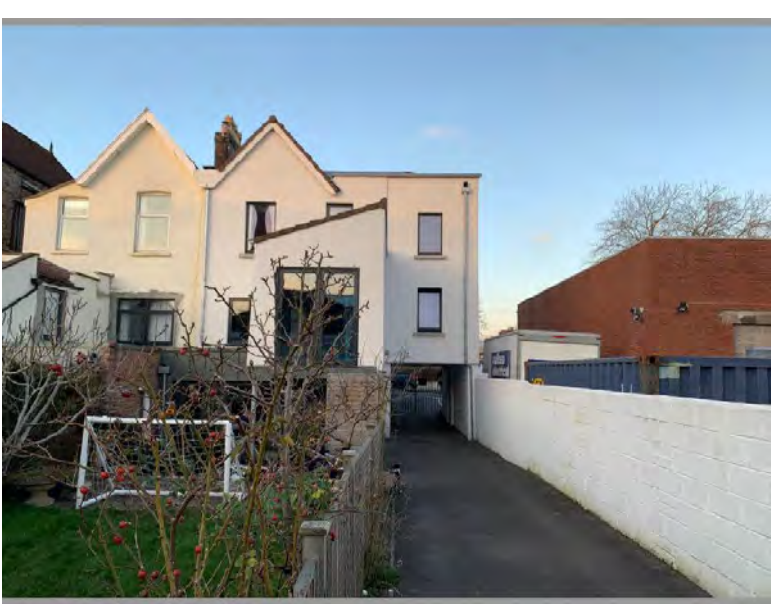
# Site Photos- Various Dates- 2019 to Present

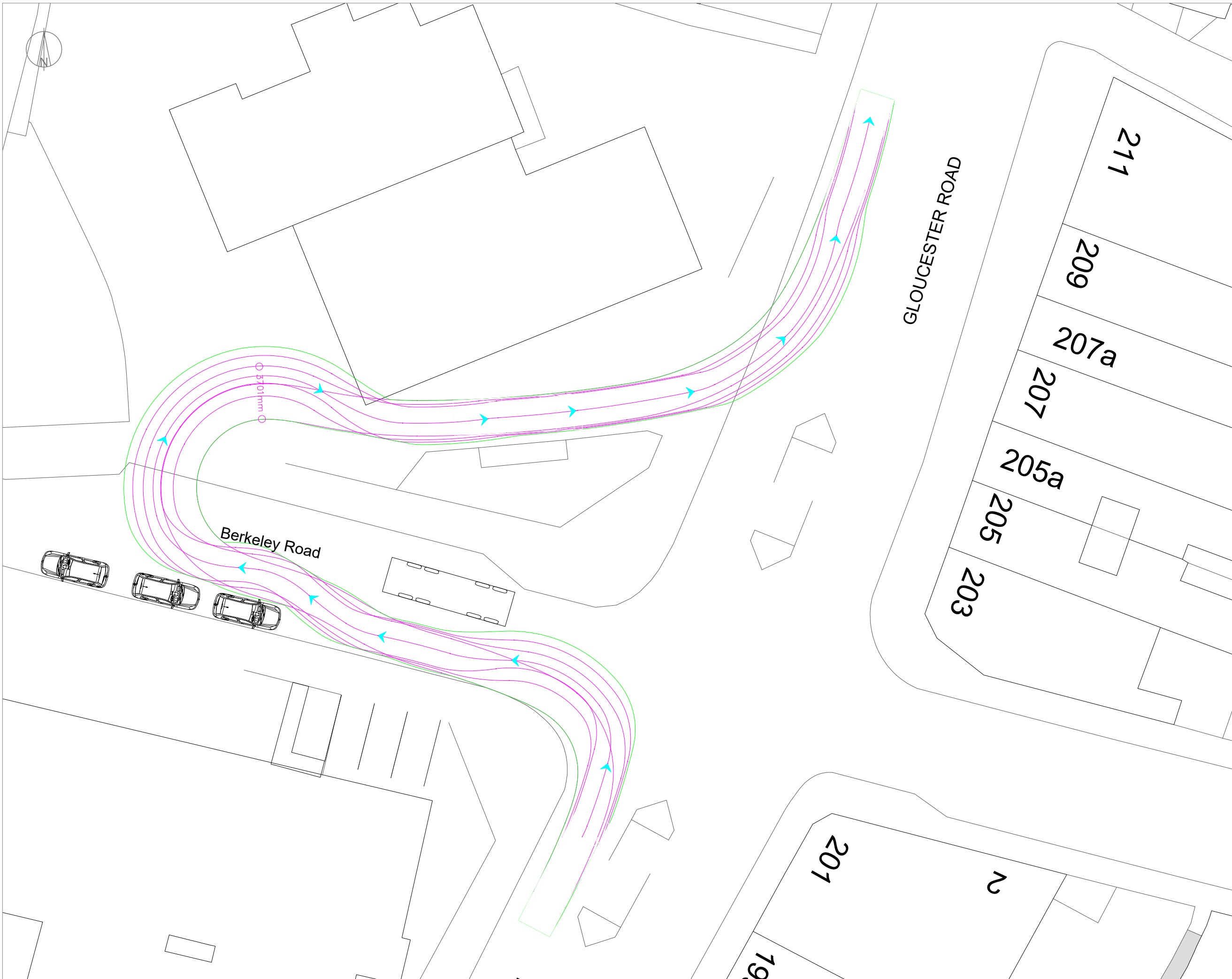






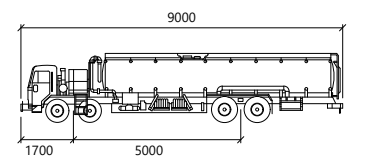






NOTES:  
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 Drawing based on David Cahill Design Consultants Proposed Site Plan 3516/10/B, dated October 2022 and provided April 2023.

| A     | Updated for officer comments | JT | 28.04.23 |
|-------|------------------------------|----|----------|
| ISSUE | REASON FOR REVISION          | BY | DATE     |



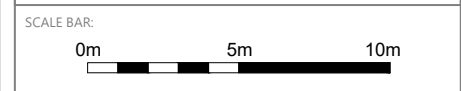
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 Track : 2500 mm  
 Lock To Lock Time : 6.0  
 Steering Angle : 39.5

PROJECT:  
 102 GLOUCESTER ROAD  
 BRISTOL

CLIENT:  
 DAVID CAHILL DESIGN  
 CONSULTANT

|                      |                         |                         |
|----------------------|-------------------------|-------------------------|
| PROJECT REF:<br>2383 | DRAWING NUMBER:<br>TR01 | SCALE (AT A3):<br>1:250 |
|----------------------|-------------------------|-------------------------|

SHEET NUMBER:  
 SHEET NUMBER 1 OF 1

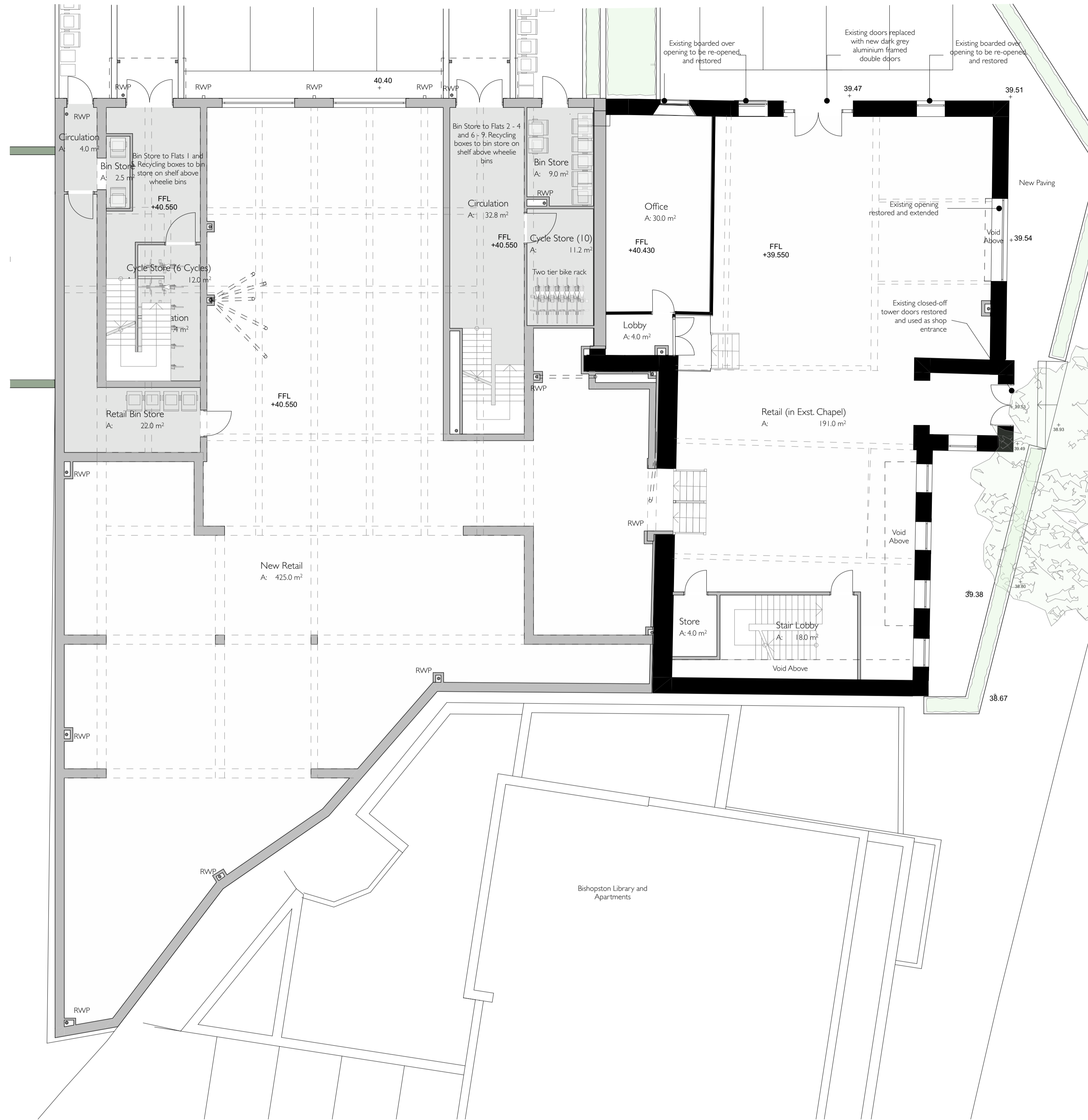


**Highgate Transportation**  
[www.highgatetransportation.co.uk](http://www.highgatetransportation.co.uk)  
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 01179 349 121  
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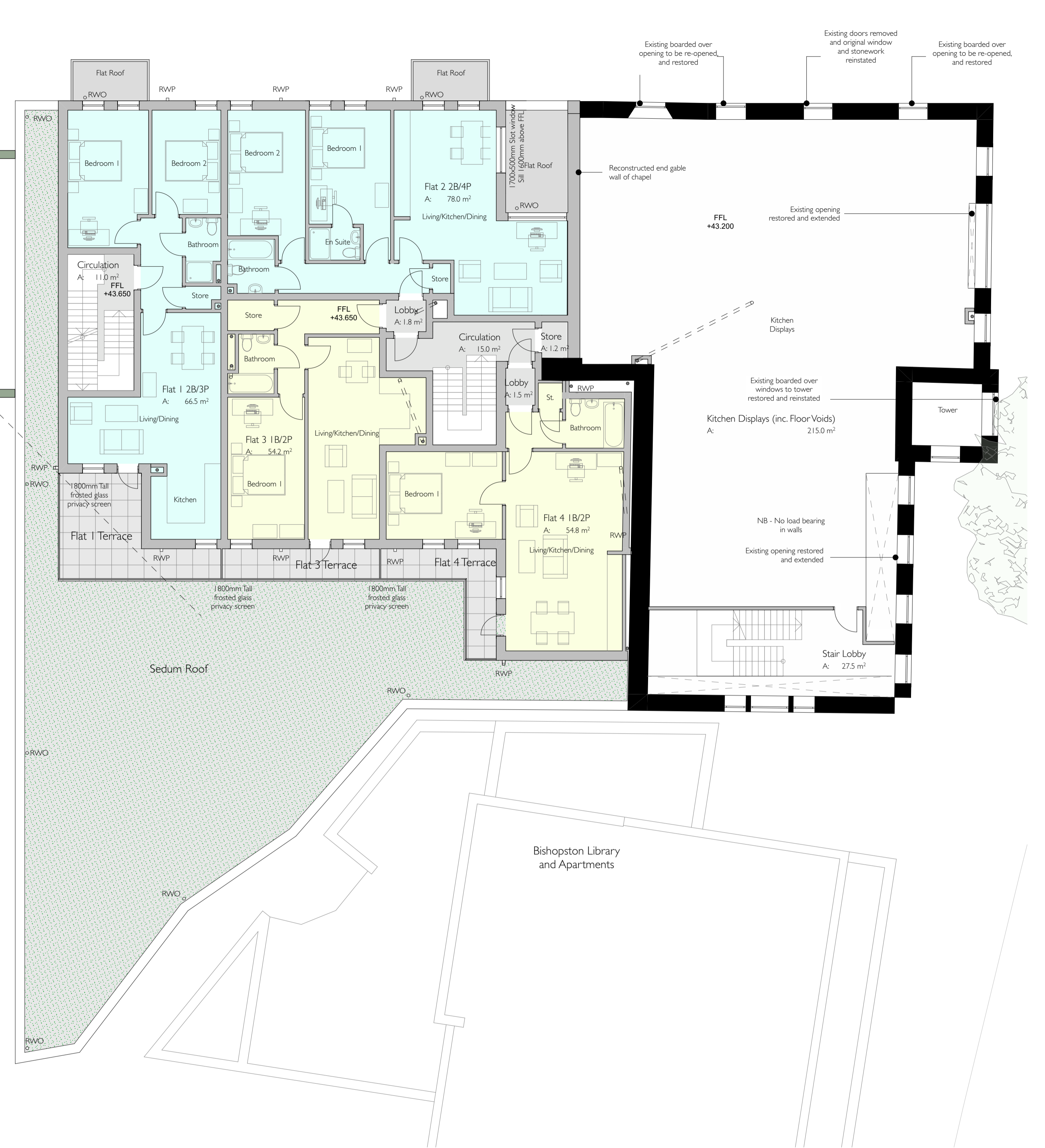
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 SWEPT PATH ANALYSIS  
 FUEL TANKER

|                   |                 |                |
|-------------------|-----------------|----------------|
| DATE:<br>25.04.23 | DRAWN BY:<br>FB | CHECKED:<br>DT |
|-------------------|-----------------|----------------|





Proposed Ground Floor Plan



Proposed First Floor Plan

PL08 Rev C  
Proposed Ground and First Floor Plans

1:100 @ A1

Nailsea Electrical, Gloucester Road, Bristol

**NICHOLAS MORLEY ARCHITECTS LTD**

Box House, Bath Road, Box, Corsham, SN13 8AA

Email: [njm@njmarchitecture.co.uk](mailto:njm@njmarchitecture.co.uk)

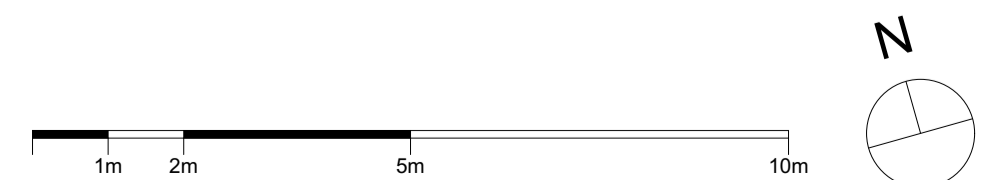
[www.nicholasmorleyarchitects.co.uk](http://www.nicholasmorleyarchitects.co.uk)

**Revision History**

Rev. C - 01.09.2020 - Amendments made in line with previous discussions with and comments from planning officer

Rev. B - 12.08.2020 - Various amendments made in line with previous discussions with and comments from planning officer

Rev. A - 20.02.2020 - Bin store notes amended and additional cycle storage area (sheffield stands) shown







Proposed Second Floor Plan



Proposed Roof Plan

PL09 Rev B  
Proposed Second Floor and Roof Plans

1:100 @ A1

Nailsea Electrical, Gloucester Road, Bristol

**NICHOLAS MORLEY ARCHITECTS LTD**

Box House, Bath Road, Box, Corsham, SN13 8AA

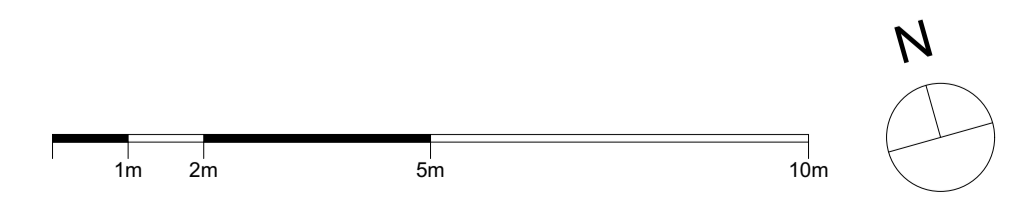
Email: [njm@njmarchitecture.co.uk](mailto:njm@njmarchitecture.co.uk)

[www.nicholasmorleyarchitects.co.uk](http://www.nicholasmorleyarchitects.co.uk)

**Revision History**

Rev. B - 01.09.2020 - Various amendments made in line with previous discussions with and comments from planning officer

Rev. A - 12.08.2020 - Various amendments made in line with previous discussions with and comments from planning officer



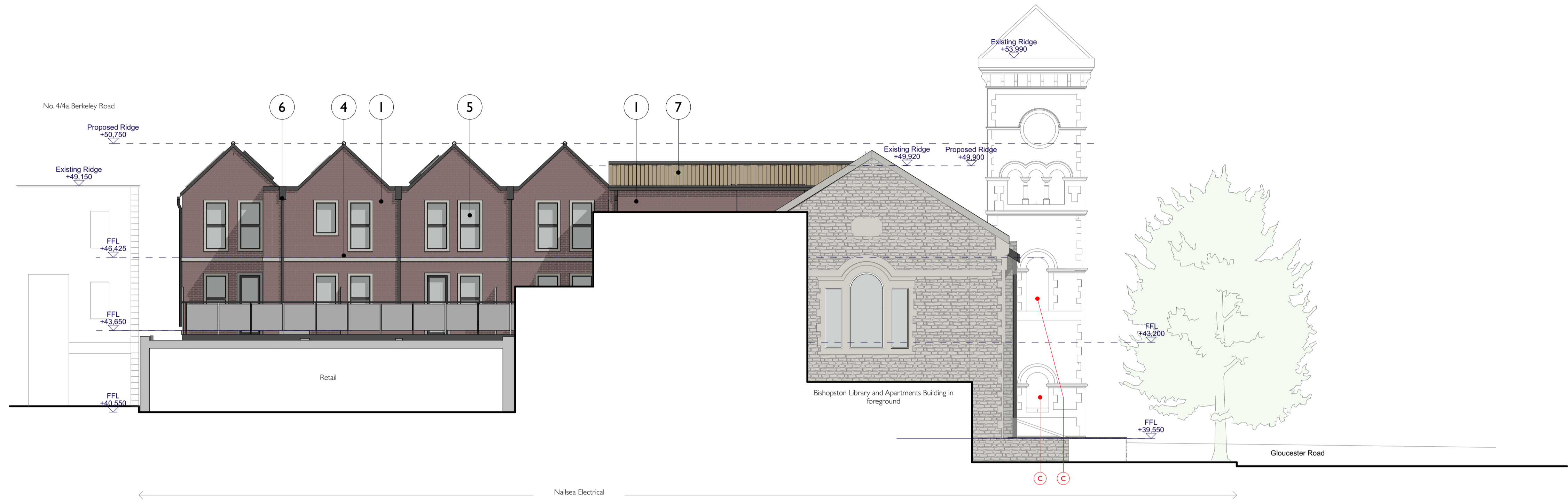
**Key for New Building Works:**

- 1 Red brickwork
- 2 Porch roof to residential entrances with flat roof with dark grey metal perimeter flashings and dark grey metal column support
- 3 Natural slate roof tiles
- 4 Bath stone banding to external walls
- 5 Dark Grey PPC Aluminium glazing system. Windows to have stone surround where indicated
- 6 Dark grey metal rainwater goods
- 7 Dark brown double roman tiles matching existing chapel
- 8 Dark grey metal signage
- 9 New pennant stone matching existing chapel with stone surrounds to new openings.

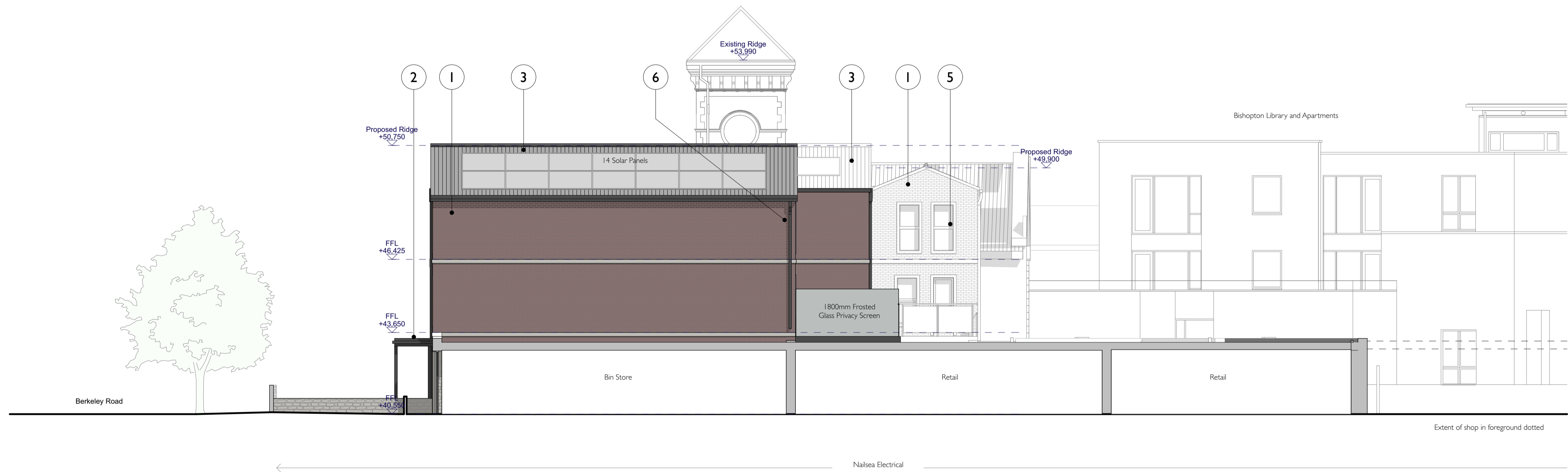
**Key for Proposed Works to Existing Chapel:**

- A Dormer window to existing Chapel to line through with existing windows below
- B New dark red/brown double roman roof tiles
- C Existing boarded up window/door to be re-opened and restored with new/repainted window frame and new glazing. Stonework window surround to be repaired and restored where needed
- D Existing opening to be restored and filled with new glazing. Size of existing opening increased
- E New rooflight in chapel roof
- F New door with glazed balustrade in existing chapel wall
- G Existing non-original doors removed and new double doors with dark grey metal frame proposed
- H New gable, coping and parapet barrier constructed. All materials to match existing chapel materials
- J Existing opened filled in with stonework to match existing

Note - All damaged/dilapidated stonework to chapel is to be repaired and restored



Proposed South Elevation



Proposed West Elevation

PL11 Rev A  
Proposed South and West Elevations

1:100 @ A1

Nalsea Electrical, Gloucester Road, Bristol

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**Revision History**

Rev.A - 12.08.2020 - Various amendments made in line with previous discussions with and comments from planning officer





**Key for New Building Works:**

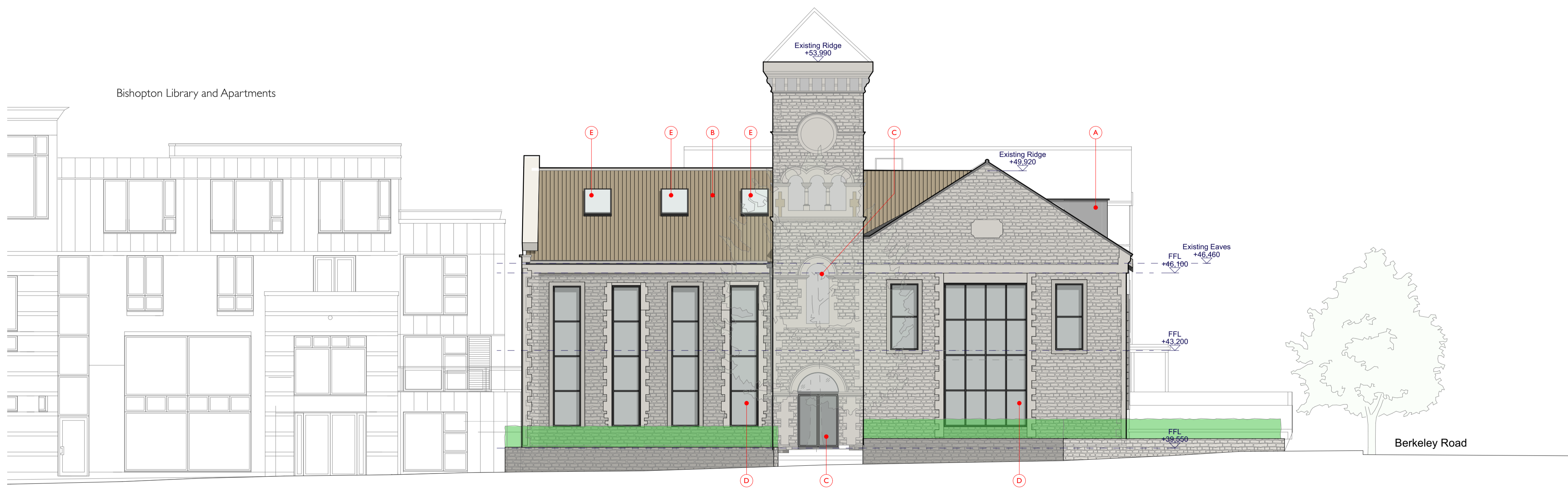
- 1 Red brickwork
- 2 Porch roof to residential entrances with flat roof with dark grey metal perimeter flashings and dark grey metal column support
- 3 Natural slate roof tiles
- 4 Bath stone banding to external walls
- 5 Dark Grey PPC Aluminium glazing system. Windows to have stone surround where indicated
- 6 Dark grey metal rainwater goods
- 7 Dark brown double roman tiles matching existing chapel
- 8 Dark grey metal signage
- 9 New pennant stone matching existing chapel with stone surrounds to new openings.

**Key for Proposed Works to Existing Chapel:**

- A Dormer window to existing Chapel to line through with existing windows below
  - B New dark red/brown double roman roof tiles
  - C Existing boarded up window/door to be re-opened and restored with new/repainted window frame and new glazing. Stonework window surround to be repaired and restored where needed
  - D Existing opening to be restored and filled with new glazing. Size of existing opening increased
  - E New rooflight in chapel roof
  - F New door with glazed balustrade in existing chapel wall
  - G Existing non-original doors removed and new double doors with dark grey metal frame proposed
  - H New gable, coping and parapet barrier constructed. All materials to match existing chapel materials
  - J Existing opened filled in with stonework to match existing
- Note - All damaged/dilapidated stonework to chapel is to be repaired and restored



Proposed North Elevation (Berkeley Road)



Proposed East Elevation (Gloucester Road)

**Revision History**

- Rev. C - 01.09.2020 - Amendments made in line with previous discussions with and comments from planning officer
- Rev. B - 12.08.2020 - Various amendments made in line with previous discussions with and comments from planning officer
- Rev. A - 20.02.2020 - Spandrel panel locations switched to adjacent window

PL10 Rev C  
Proposed North and East Elevations

1:100 @ A1

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